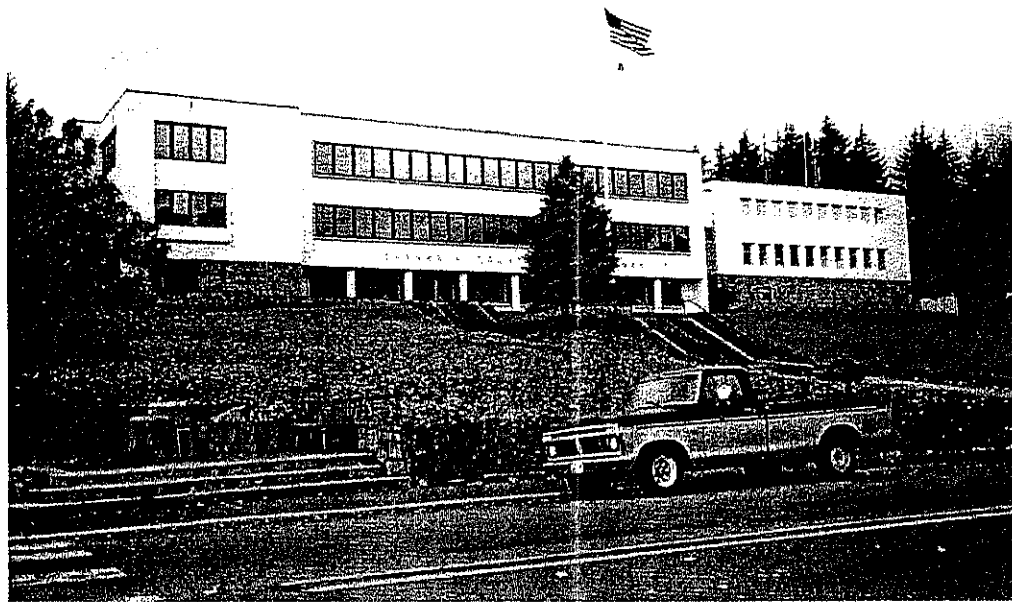


DOWNTOWN STEVENSON



DRAFT DEVELOPMENT FRAMEWORK, STANDARDS & DESIGN GUIDELINES

August 1999

CITY OF STEVENSON, WASHINGTON

CRANDALL ARAMBULA PC

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DEVELOPMENT REVIEW CHECKLIST

Introduction

Intent

Downtown Development Goals & How to Use
Submittal Requirements

Design Review Process & Major/Minor Projects

Intent

Stevenson’s downtown is the center of Skamania County’s retail, commercial, and governmental activities and continues to grow as a destination for tourists and recreationalists. Over the last few years, concerted efforts have been initiated by local officials, community leaders and business owners to increase the vitality and viability of downtown businesses.

The following Development Framework, Standards and Design Guidelines ensure that new construction or renovation of existing buildings is:

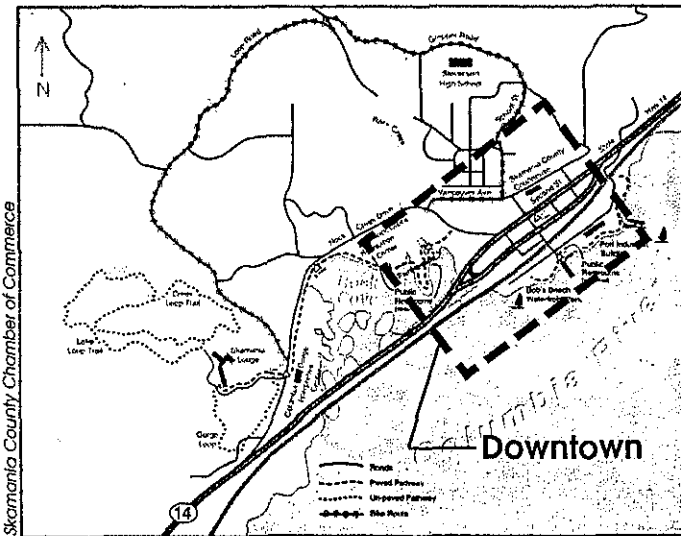
- Consistent with the historic character of Downtown Stevenson.
- Of high quality and long lasting materials.

The Development Framework, Standards and Design Guidelines will:

- Assure that each development site is consistent with Stevenson’s Framework Plan and existing zoning.
- Assure specific descriptive requirements and guidelines for streetscape improvements and development site planning, architecture, lighting, and signs are addressed.
- Ensure the urban character of downtown is maintained without dictating or prescribing a specific architectural style.
- Be easy to use by both developers and reviewing government agencies.



Second Street - The retail artery of downtown



Vicinity Plan

Downtown Development Goals

- Create a pedestrian-friendly downtown.
- Increase commercial and retail development.
- Increase residential development.
- Use existing downtown infrastructure, open space and buildings.
- Integrate and enhance the value of adjacent natural features such as the Columbia River and views across and along the Gorge.

How to Use

The Development Framework, Standards and Design Guidelines apply to new construction and major renovations or replacement of existing lighting or signs within the downtown plan area. This area extends from the intersections of First and Second Streets to the east and west, and from Vancouver Avenue to the Columbia River.

Development Framework

The Development Framework serves as a basis for the Development Standards and Design Guidelines. It establishes a land use and circulation framework for future development and rehabilitation and re-use of properties within downtown Stevenson.

Development Standards

The Development Standards supercede the existing Development Code where discrepancies exist. They are regulatory requirements which are not negotiable during the Design Review Process.

Design Guidelines

Design Guidelines support the Development Framework and Development Standards. They apply to all development that occurs downtown, and are administered through the design review process. The Commission may waive individual guidelines for specific projects based on findings that the waiver will better accomplish the goals. Should the Stevenson Planning Council find that one or more design guidelines are not applicable to the circumstances of the particular project being reviewed, the guideline(s) may be waived by the Stevenson Planning Council.

The Stevenson Planning Council may also address aspects of a project's design which are not covered in the guidelines where the Planning Council finds that such action is necessary to better achieve Stevenson's downtown development goals.

Submittal Requirements

Applications for design review may be obtained from the City of Stevenson. An application for review by the Stevenson Planning Council must be accompanied by a set of drawings including the following:

New Construction Projects

- A site plan including the shape and location of all buildings and major land uses within 200' of the project site perimeter.
- A complete set of building elevations, isometrics and/or perspectives. (A list of materials and color to be used must be indicated on drawings. Samples may be submitted.)
- A parking plan showing the location of all on-site parking and loading facilities, including access and egress routes.
- A ground floor plan showing all grade changes and indicating the use of various spaces.
- Typical floor plans as necessary to adequately describe all levels of the building.
- Sections of the project or building to adequately describe the shapes and relationships of spaces.
- A landscape plan showing all planting areas, street furniture, street trees, outdoor art, and other outdoor features of the project, including a list of materials and colors.

- Sign plans showing all sizes, materials and locations on the building or site
- Lighting plan showing all types, materials and locations on the building or site
- Identification on site plan of location and species for all trees with a 5" or greater caliper measurement. Trees to remain as well as trees designated for removal are to be included. Historical significance of trees and project impact will be reviewed by City Staff. Additional assessment of impacted significant trees by an arborist may be required and mitigation measures to be included in Design Review application may be required.

Remodeling or Rehabilitation Projects

- All of the new construction requirements that are relevant as determined by city staff

Design Review Process

Criteria

Design Review does not require that every decision maker become skilled in design and design analysis. It does demand that the people involved in important design decisions recognize and enforce the strong organizing patterns that currently exist. The Design Guidelines let decision-makers know what to look for and provides them the criteria to objectively consider and evaluate project proposals.

Compliance with the Design Guidelines can take many forms for a given project. The guidelines are meant to state broad design objectives and shall not be construed as prescriptive standards.

Preapplication Meeting

Project designers and developers are strongly encouraged to request an early briefing with the Stevenson Planning Council and/or City Staff prior to formal application for a hearing. Such meetings provide an opportunity for informal discussion of the specific circumstances of the project and how the Development Framework, Standards and Design Guidelines might effect its development.

Major and Minor Projects

Proposals reviewed by the Stevenson Planning Council will be classified as major or minor projects.

Minor Projects: A minor project is one that the Planning Director and the Planning Council Chairman find will not significantly affect the character, use, and development of the surrounding properties. Minor projects will be reviewed for consistency with the Development Framework, Standards and Design Guidelines by the Planning Director and staff.

Major Projects: Major projects as determined by chairman will be reviewed by the Planning Council for consistency with applicable design guidelines.

Upon receipt of an acceptable application adequately describing the project, a hearing on the proposed project will be scheduled. The application must be received at least 10 days before the next regularly scheduled Planning Council Meeting. Upon receipt of an acceptable application adequately describing the project, a hearing on the proposed project will be scheduled at the next Planning Council Meeting.

Should an applicant object to any aspect of a decision by the Design Commission or City Staff, they have 20 days in which to file an appeal with the Stevenson Boundary Review Board and subsequently with Superior Court.

Development Framework

Framework Concept & Development Framework Features

Development Framework Diagram

Circulation Framework Features

Circulation Framework Diagram

Build-To Line

Active Street Frontage

Rain Protection

Build-To Line, Active Street Frontage & Rain

Protection Diagram

Framework Concept & Development Framework Features

To foster an economically viable downtown, enhance Stevenson's character and preserve its heritage for future generations, the following pages describe a land use and circulation framework for future development and rehabilitation or re-use of properties within Downtown Stevenson. Often land use and transportation improvements are made without an overall plan or recognition of the relationship to adjacent neighbors. This can lead to conflicts between competing uses.

Compatible Uses

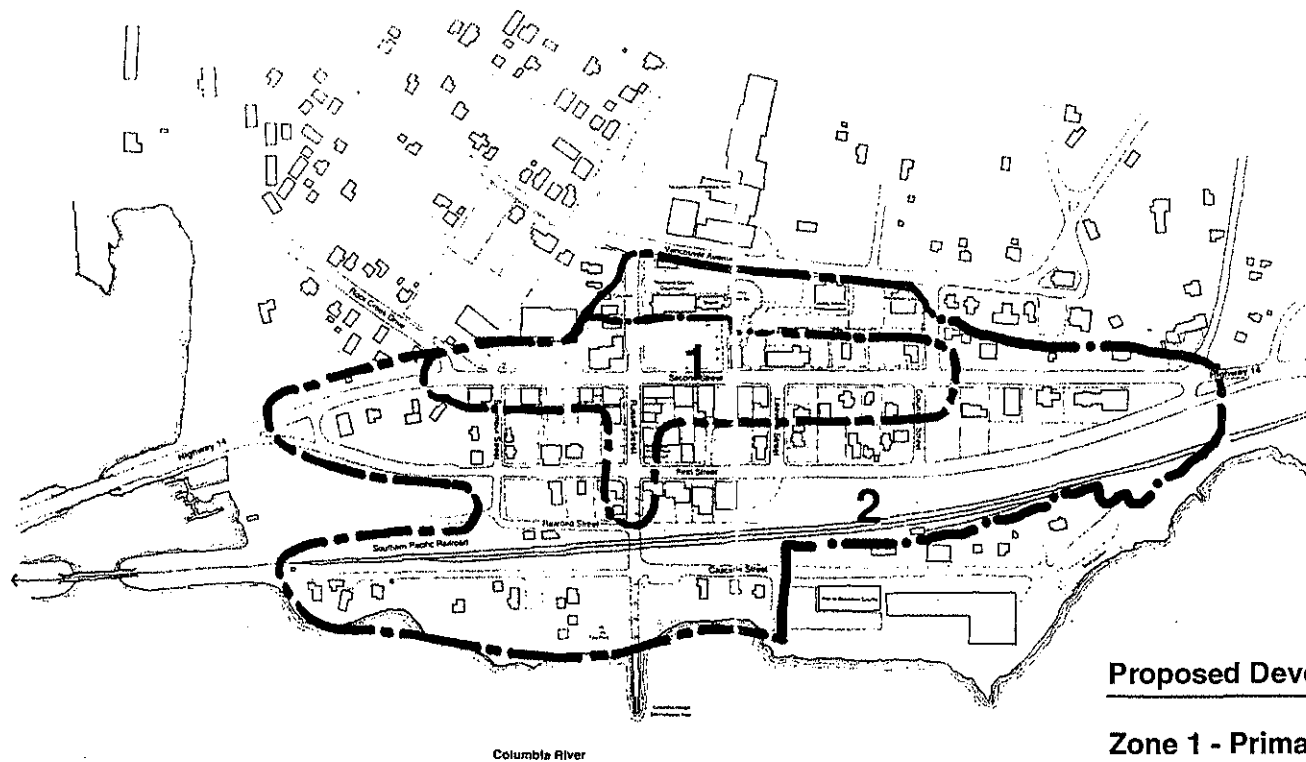
The Framework Plan for Downtown Stevenson prescribes a "game plan" so that future land use, transportation and open space improvements are built in a manner that is respectful and complementary to its context, thereby creating an understandable, distinctive and memorable downtown.

Confidence and Certainty

The Downtown Stevenson Framework Plan establishes the environment in which developers and existing business people can be assured that Stevenson is a good place to do business. "Zone 1" is the primary retail zone and "Zone 2" is the supporting commercial zone. The framework:

- Maximizes current investment in buildings and infrastructure.
- Supports and strengthens Second Street as the primary retail armature (Zone 1).
- Suggests that retail uses along Russell Street should be fostered to tie the Columbia River into downtown.
- Recognizes that there is a limited marketplace. The framework suggests that First Street be a secondary "mixed use" commercial street that does not compete with Second Street, but rather complements it with supporting services.
- Suggests that waterfront business along Cascade Street should foster and support tourism, industry and recreational uses.
- Suggests that civic uses should be limited to Vancouver and Russell Streets (north of Second Street).
- Promotes additional residential uses, especially townhouse and apartment development in the downtown area.
- Promotes the development of civic spaces and attractors - i.e. Courthouse Square.

Development Framework Diagram



Proposed Development Zones:

Zone 1 - Primary Retail Corridor

Zone 2 - Supporting Commercial and Service Uses

Circulation Framework Features

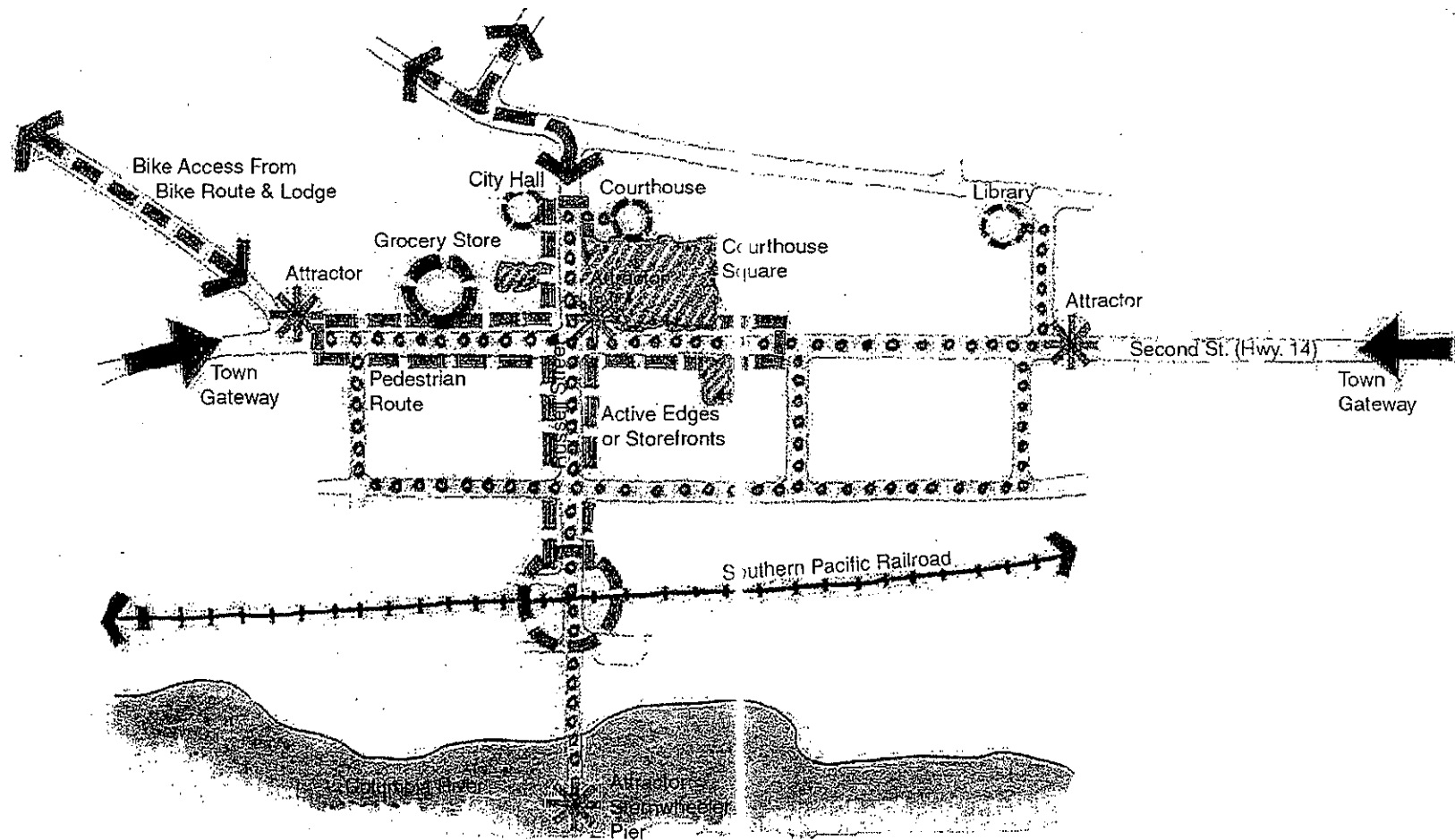
The Pedestrian is the Priority

The pedestrian must have the highest priority in the network of roadways, pathways and open spaces that cross and connect downtown. This network can be simply described as an environment where a humane, walkable cityscape has been preserved or created without compromises. However, this does not preclude the importance of providing motor vehicle access to maintain downtown as the center of commerce and civic life.

The public investment in infrastructure serves as a catalyst for change and an impetus for private sector investment. The Framework Plan provides a blueprint for the implementation of positive changes in the appearance, livability and safety for residents, downtown employees and visitors. The framework:

- Bridges all existing pedestrian obstacles and provides a clear and understandable connection to the Columbia River.
- Provides pleasurable areas for strolling, resting, watching, eating and socializing.
- Considers and incorporates elements that respond to the environment, especially the winter season.
- Is barrier-free and accessible to people of all ages and physical conditions.
- Slows truck and auto traffic downtown by incorporating “traffic calming” devices such as curb extensions.

Circulation Framework



Build-To Line

Buildings along designated streets must be built out to the property line with no setbacks allowed to ensure design consistency with historic structures, and to promote a sense of urban enclosure. This contrasts to suburban environments where buildings do not engage the street causing gaps and discontinuities, eroding the retail environment at the street line and destroying any sense of urban enclosure.

Required:

- 0' front yard setback (see yard requirements, Development Standards Matrix, Page DS-3)
- Recessed doorways set back (5' maximum), especially at corners
- Recessed windows (8" maximum), especially at groundfloor

Prohibited:

- Plazas or landscaped forecourts
- Auto/Service drop-offs or pullouts



Appropriate: Retail building with no setback, open and engaging storefronts, and awnings



Not Appropriate: Retail building set back to provide angled parking in front erodes street life and an interesting visual environment for pedestrians

Active Street Frontage

The design of the ground floor of buildings along designated active streets should be open and inviting, engage the pedestrian, and help create activity along the sidewalk and street. Wide openings between buildings, parking lots, and impenetrable blank building walls and lack of windows and entries discourage pedestrian use of the sidewalk and compromise the overall activity of the street. Active streets and sidewalks are created when buildings are continuous, inviting, and employ quality materials and detailing, especially at the ground floor.

Required:

- Primary building entry along street
- 50% (minimum) glazing at groundfloor street frontages
- Storefront windows

Prohibited:

- Blank walls
- Tinted, reflective or opaque groundfloor glazing



Not Appropriate: "Dead" walls with no



Appropriate: Large cafe windows and outdoor seating



Appropriate: Large retail windows and rain protection

Rain Protection

Awnings & Canopies

Awnings and canopies will encourage pedestrians to comfortably use sidewalks during inclement weather. They should be designed as an integral component of the building facade.

Required:

- Canvas fixed or retractable awnings
- Horizontal metal canopies

Prohibited:

- Vinyl or other synthetic fabrics
- Backlit awnings
- Oversized advertising or tenant signs on awnings



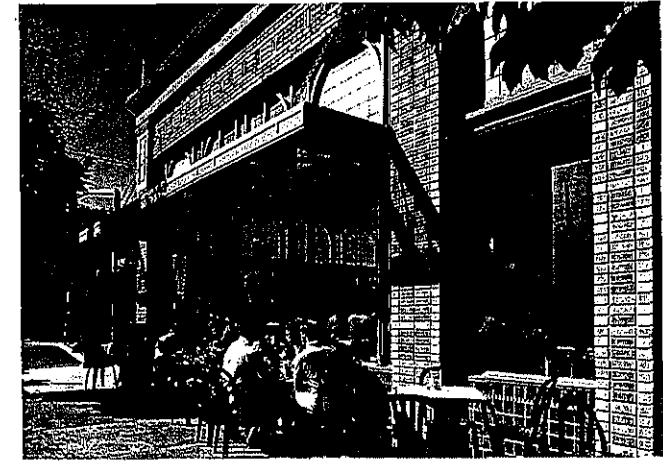
Appropriate: Retractable fabric awnings



Inappropriate: Vinyl awnings

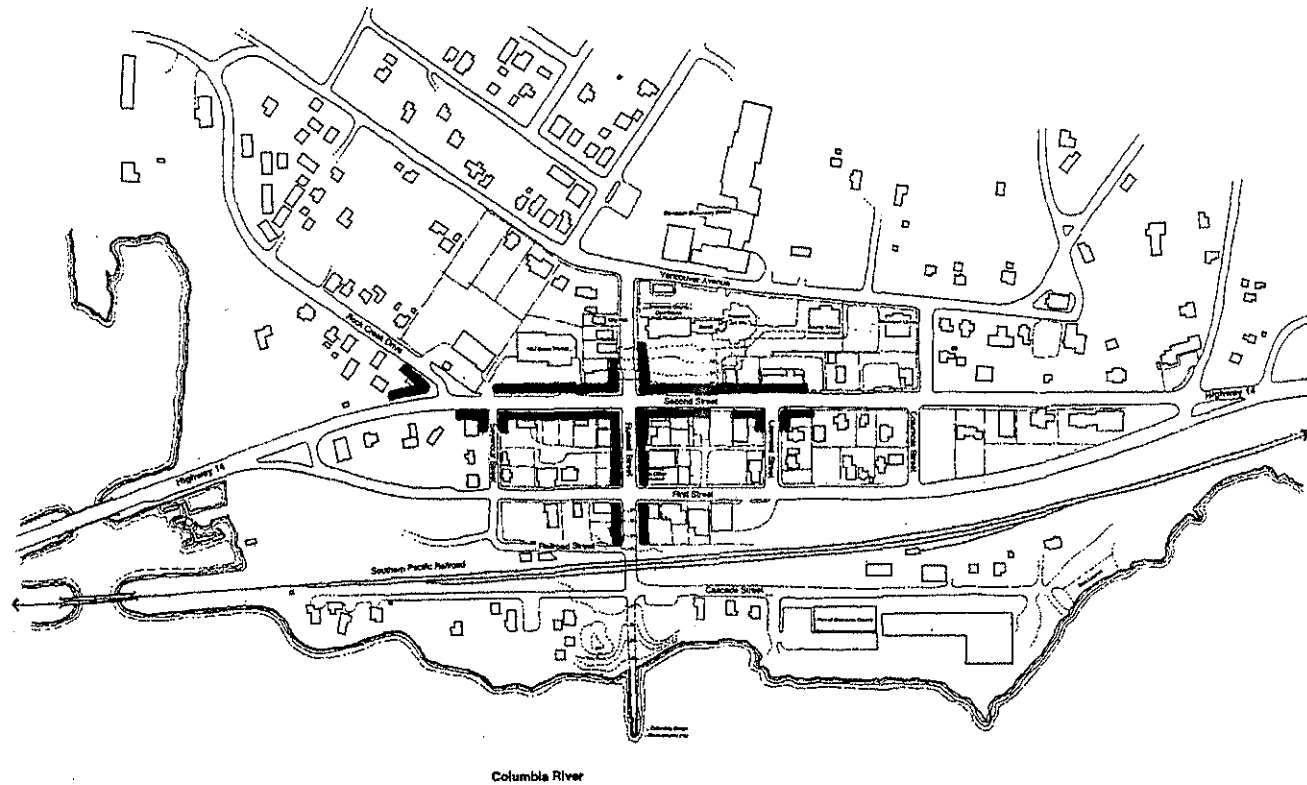


Appropriate: Space heaters mounted below awnings



Appropriate: Canopies

Build-To Line, Active Street Frontage and Rain Protection



█ Build-To Line, Active Street Frontage and Rain Protection Required

Design Guidelines - Public Areas

Purpose

Landscaping

Landscaping Diagram

Street Lighting

Street Lighting Diagram

Sidewalks

Sidewalks Diagram

Sidewalks Sections

Curb Extension

Pedestrian Crossing

Pedestrian Crossing Diagram

Street Furniture

Street Furniture Diagram

Railings and Fences

Retaining Walls

Purpose

The streets and public spaces within Downtown Stevenson should not be considered simply as conduits for traffic to move through. Rather, they are enclosed “outdoor rooms” that provide for auto, bicycles, trucks and pedestrians alike. An environment should be fostered that encourages window shopping, strolling and sitting. To improve the downtown public area, the Design Guidelines:

- Preserve the riverfront as a public amenity and provide convenient, direct and safe access.
- Require new development that is more pedestrian friendly in character by promoting development that is compact and less auto dependent.
- Promote the use of unifying elements that strengthen downtown, by adding new elements that unify and connect development zones, individual facades and blocks.
- Enhance, embellish and identify the downtown as a unique place. Incorporation of unique, small scale features add to downtown’s ambiance. The elements must consider and reinforce Downtown Stevenson’s history and traditions.
- Establish and maintain a sense of urban enclosure. The right-of-way should contribute to the sense of human scale by enclosing the street edge with a continuous building wall built out to the property line. Such enclosures contribute to Stevenson’s pleasant, rich and diverse pedestrian experience. When the right-of-way rooms are not defined, the sense of urban enclosure is lost.
- Strengthen gateways. The guidelines promote gateways that provide a graceful transition into downtown, providing a sense of welcome and civic pride.
- Recognize and enhance the pedestrian system. Promotes the establishment of different zones of a sidewalk, curb, street furniture zone, walking and window shopping zone.
- Protect the pedestrian from vehicles, bicycles and the environment.
- Provide stopping and viewing places.
- Foster plazas, squares and open spaces that are inviting and enjoyable. Discourages overscaled, lifeless and difficult to maintain spaces.
- Integrate barrier-free design. Promotes design that is integral to the right-of-way rather than merely meeting minimum code standards.

Landscaping

Intent

Landscaping should beautify streets and public spaces, promote pedestrian activity, unify the downtown by providing common elements, and establish places for public assembly.

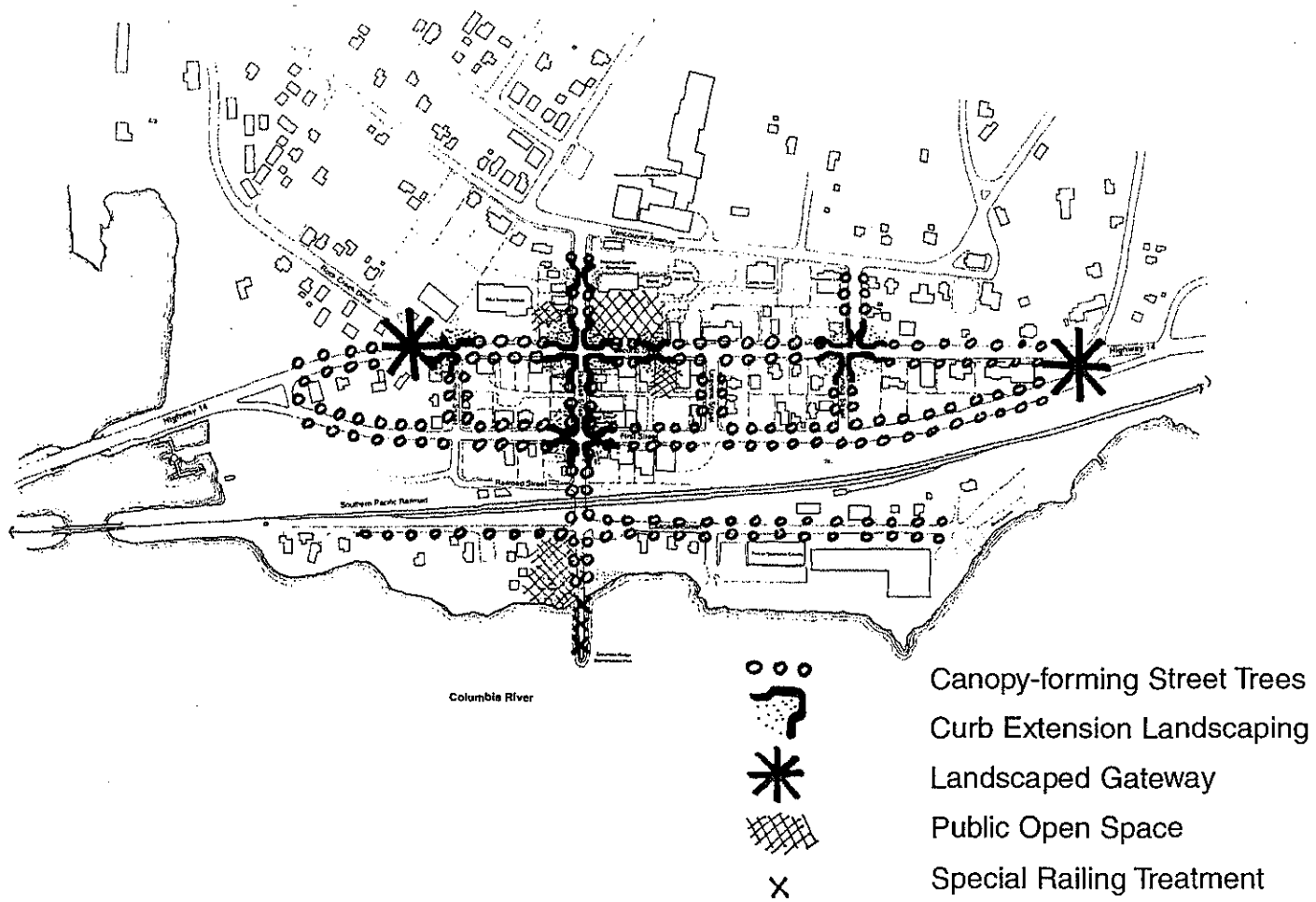
Appropriate:

- Rows of street trees that reinforce street enclosure, establish a rhythm and unify downtown with common elements.
- Shrubs and groundcover that soften and reduce the impact of paved surfaces.
- Plant materials selected to be dynamic and changing. Color during all seasons - fall, winter, spring, and summer.
- Long lasting and vigorous plants
- Minimal maintenance - Plant materials that require little irrigation, pruning and drop few leaves, seeds or fruit.
- Seasonal, annual planting beds, pots or baskets that add color and fragrance.
- Large stonework, particularly indigenous stones and boulders, integrated into landscaping.

Inappropriate:

- Exotic materials
- Obstruction of sight lines at intersections. Branch height of canopy trees should be sufficient to avoid clipping by trucks.

Landscaping



Street Lighting

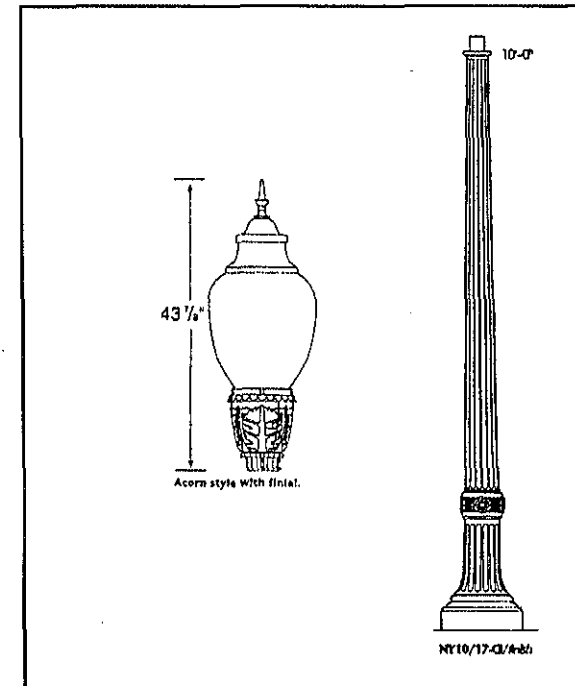
Intent

Street lighting should be designed to provide a safe environment for both motorists and pedestrians. Historic ornamental fixtures that are in character with downtown architecture and are compatible with the street furniture should be used.

Street lighting should provide warm “halo-like” glow that accents and provides interest and depth to adjacent buildings. It may be a fixture that uplifts the branches and leaves of trees.

Street lighting should be pedestrian-scale (15' standards) and spaced at regular intervals (30').

Pole standards should be black or a very dark green color. Standards may accommodate banners and hanging flower pots. Automatic drip irrigation for the pots should be considered.



Appropriate: Historic style lamp and post, scaled to pedestrians.

Street Lighting

Appropriate:

- Historic street lights and parking lot lights
- Combined light fixtures, traffic signals and pedestrian signals
- Pedestrian-scaled height -15'

Inappropriate:

- Cobra-head type fixtures
- Contemporary fixtures



Appropriate: Pedestrian scaled street lights



Not Appropriate: Cobra head fixture



Not Appropriate: Contemporary light fixtures

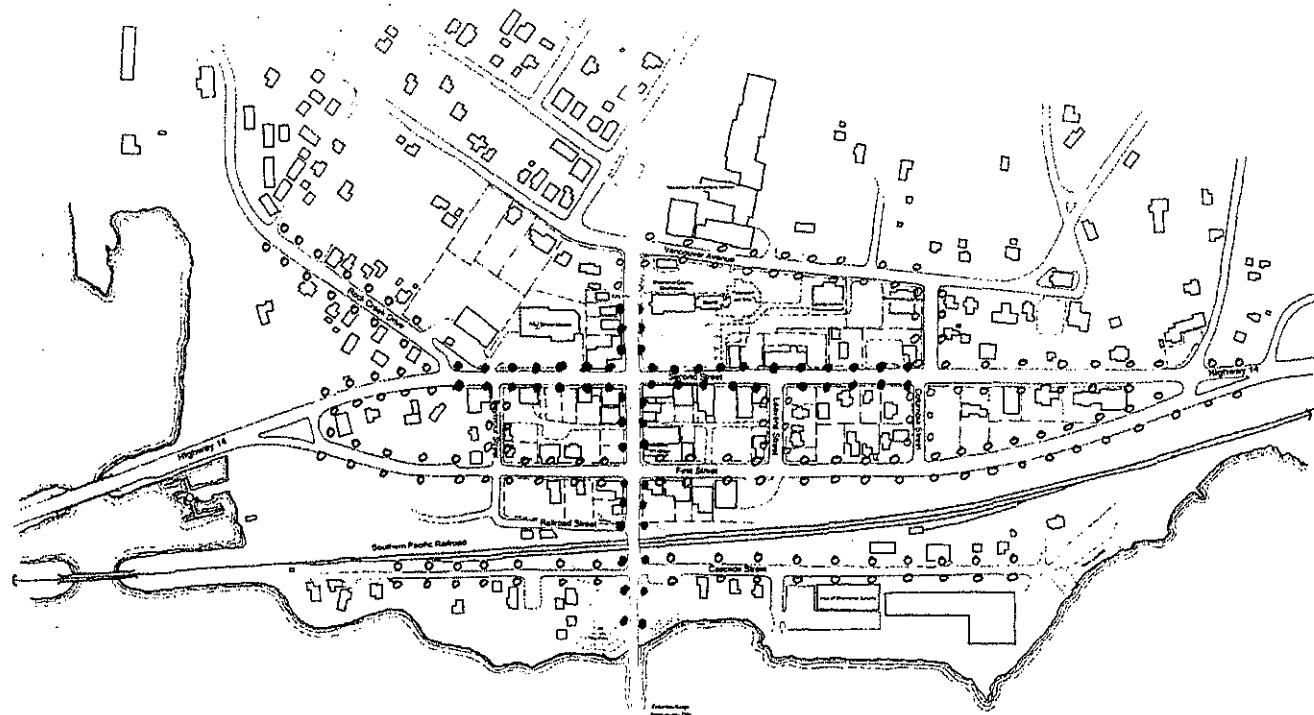


Not Appropriate: Low, contemporary fixtures



Not Appropriate: Contemporary adaptations of traditional fixtures

Street Lighting



Columbia River

- • • Priority Street Lights
- ○ ○ Secondary Street Lights

Note: Secondary Street Lights are indicated at the pathway system.

Sidewalks

Intent

The key to developing a successful Downtown Stevenson is to make the pedestrian environment the highest priority. Towns and cities that emphasize the speedy movement of automobiles at the expense of the pedestrian environment are usually lacking economically, environmentally and aesthetically. A positive experience for pedestrians enhances the economic vitality of the downtown and improves the quality-of-life for all.

A pleasant environment for pedestrians begins with the sidewalk. Essential components include a network of contiguous sidewalks that are wide enough for walking, street furniture, and trees. To grow properly, canopy streets trees need sidewalks that are at least 10' feet wide. When mature, trees break down the scale of the street, clean the air, provide shade, and improve the look and feel of the street. Grates provide the best base for street trees and discourage weeds and the accumulation of trash and debris.

The visual impact of sidewalk paving should be broken up to give it a sense of scale. A grid of joint lines (maximum 2-4 feet) achieves this as well as bricks or pavers which provide sidewalk texture.

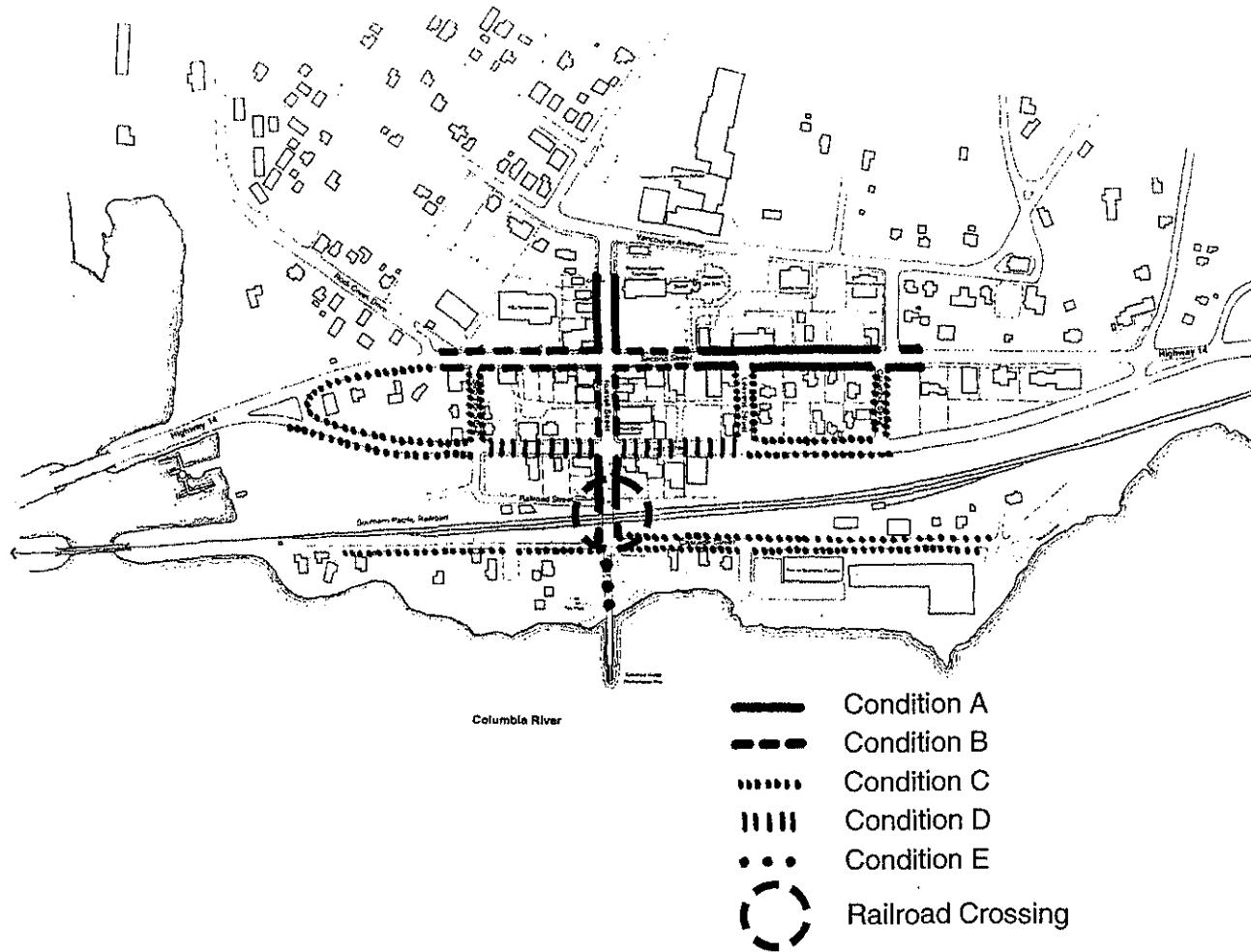


Appropriate: Gridded concrete sidewalk with street trees in square grates and a bike rack



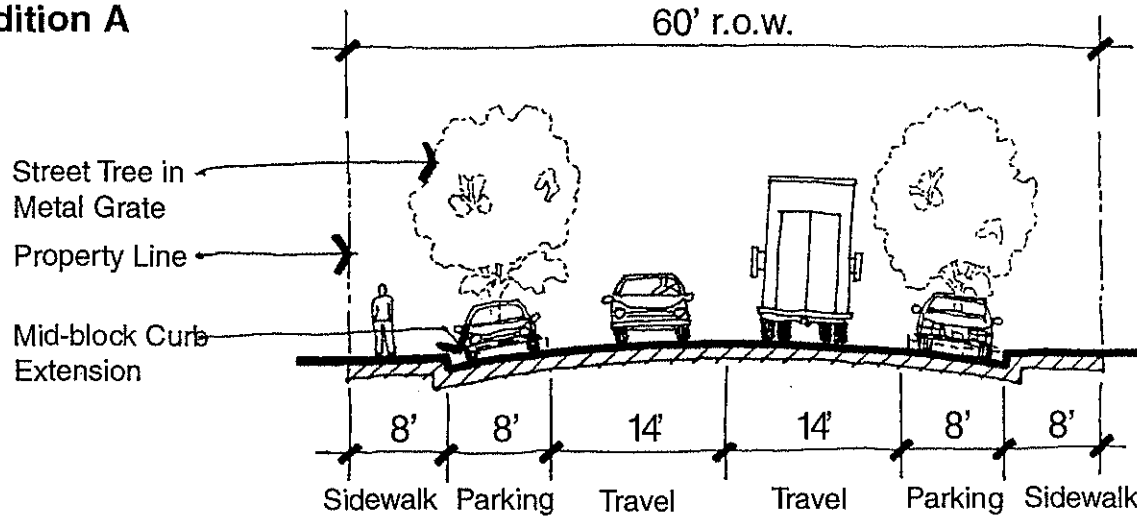
Not Appropriate: Narrow sidewalk without an adequate area for street trees

Sidewalks

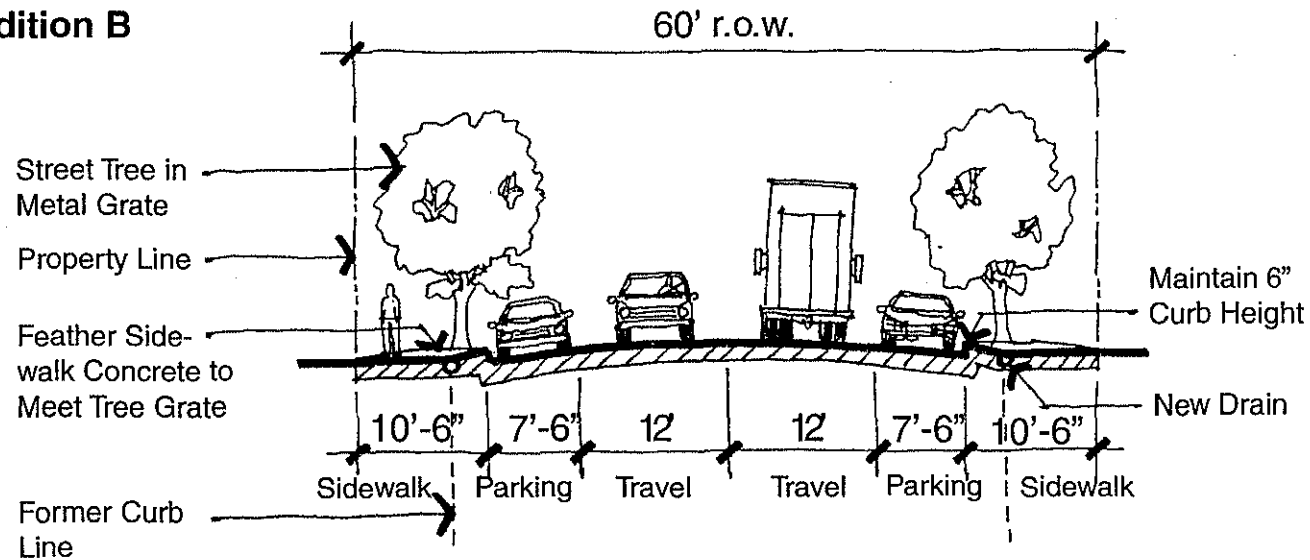


Sidewalks **44' typical curb-to-curb width to be verified

Condition A

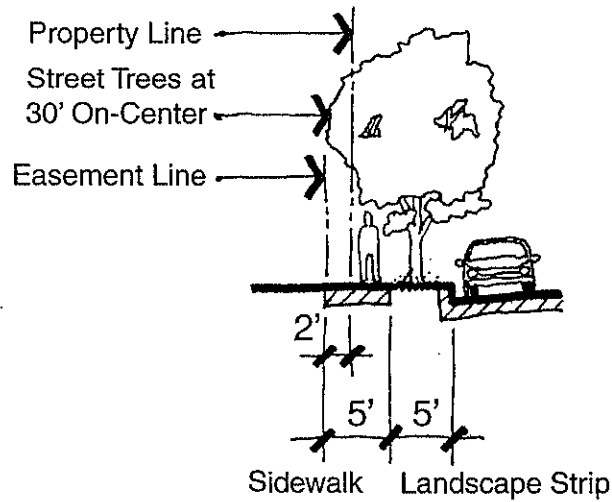


Condition B

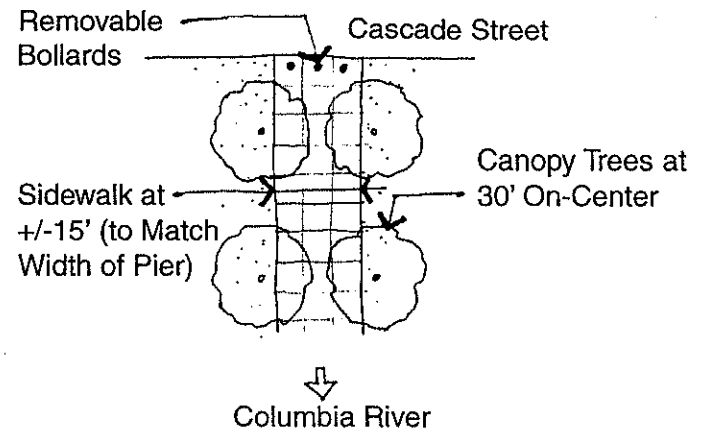


Sidewalks ****44' typical curb-to-curb width to be verified**

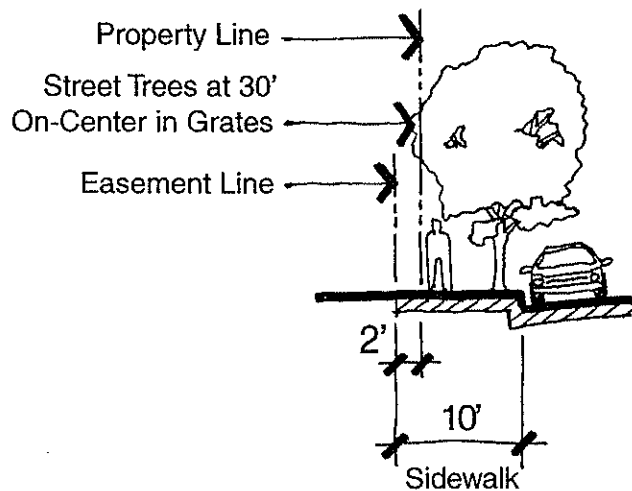
Condition C



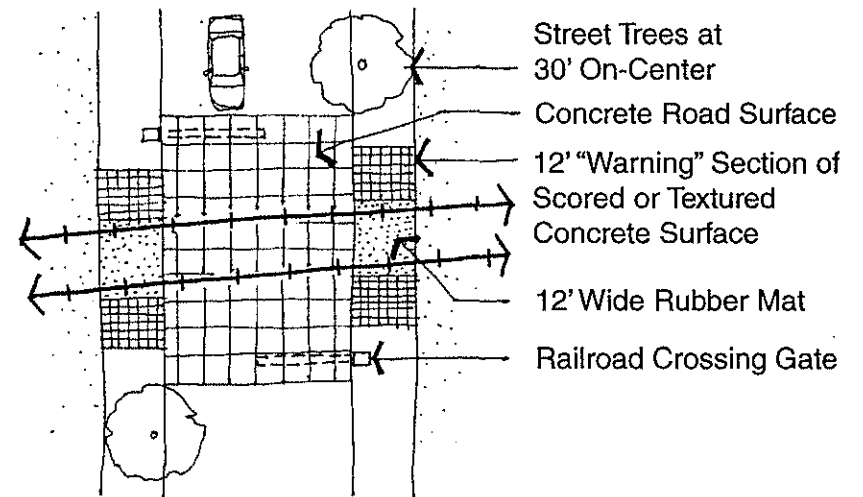
Condition E



Condition D



Railroad Crossing



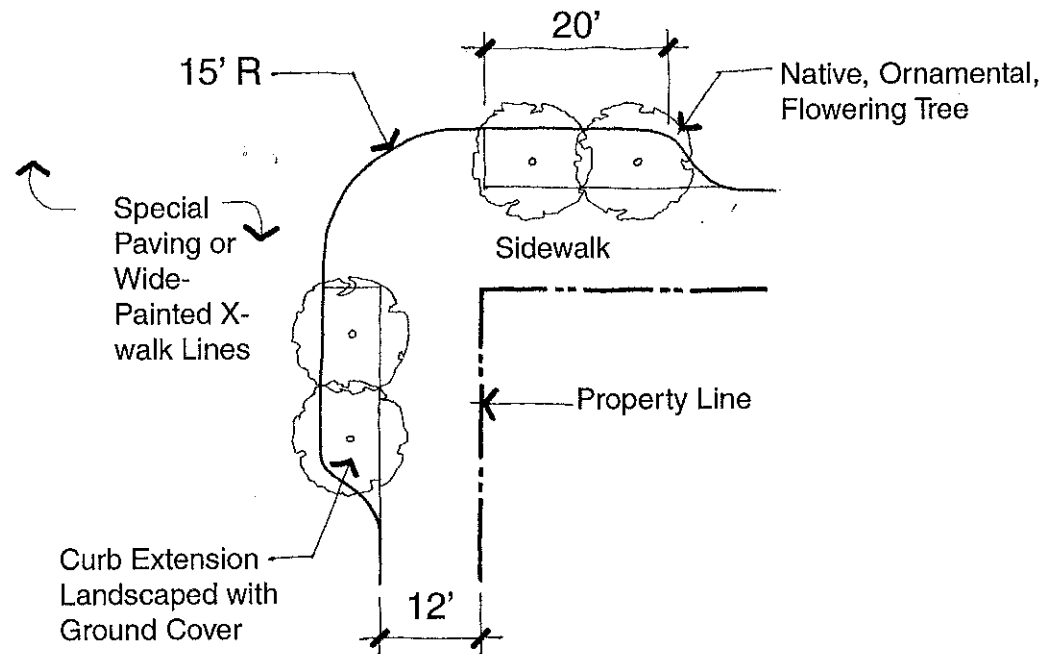
Curb Extensions

Intent

Curb extensions are an important part of improving the downtown environment for pedestrians. Extending the curb edge reduces the crossing distances for pedestrians (especially important for children and the elderly). Extensions walking “loops or circuits” within downtown, thereby encouraging shoppers to cross the street and patronize other businesses on both sides of the street.

The curb extension must include ornamental, flowering street trees and low shrubs, creating special landscaped intersections. The trees, along with the narrowed roadway at the intersections acts as a traffic-calming device. Drivers perceive a tighter space to pass through and slow down while passing through an intersection with curb extensions on all four sides. Landscape materials must be carefully selected to avoid impacting sight lines.

Curb Extension Detail



Pedestrian Street Crossing

Intent

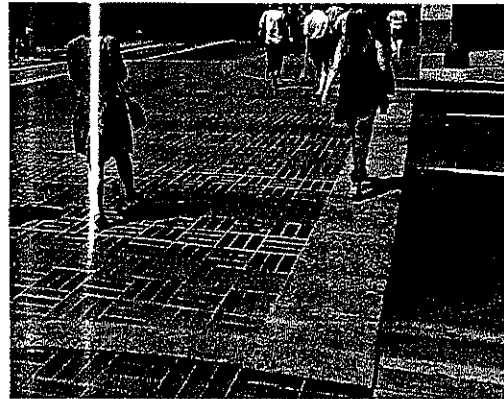
Crosswalks should minimize the intersection as a barrier to pedestrian movement. The street crossing should be clearly defined and highly visible. The crossing should be a minimum of 10' in width to accommodate numerous pedestrians.

Appropriate:

- Wide striped, reflective painted or applied strips parallel to roadway
- Special paved brick or tinted concrete surfaces
- Durable and low maintenance materials

Inappropriate:

- Deeply scored or textured materials that may impede those who are physically challenged
- Simulated brick or stone from stamped and tinted concrete appears inauthentic, lacks durability, typically provides a lumpy surface that is unsuitable for pedestrian crossings, and is not compatible in a historic district.



Appropriate: Brick sidewalks clearly delineate the automobile zone vs. the pedestrian zone.



Not Appropriate: Simulated brick or stone from stamped and tinted concrete

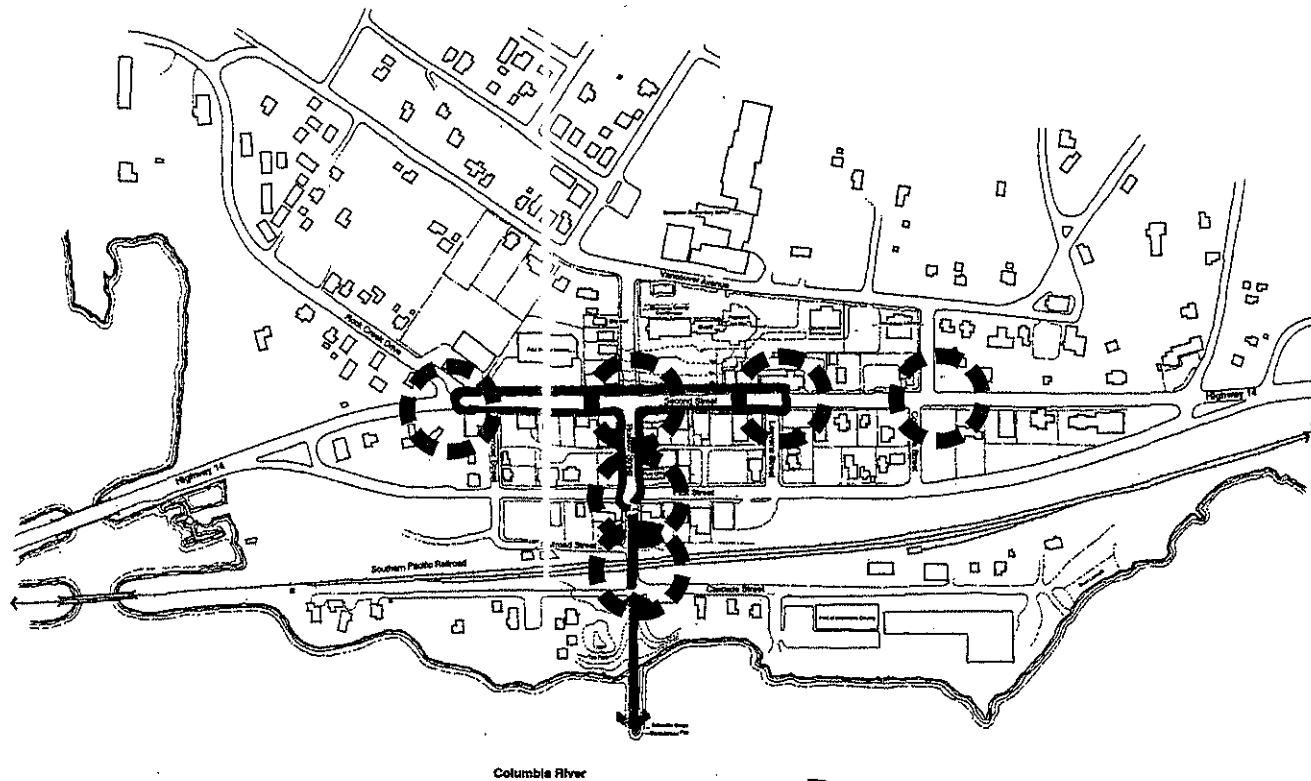


Appropriate: Wide stripes parallel to traffic movements reinforce the zone reserved for pedestrians



Not Appropriate: Single stripes wear out in heavy traffic and only minimally define the pedestrian zone

Pedestrian Street Crossing



Special Pedestrian Crossing

Street Furniture

Intent

Street furniture should be provided to encourage activity and interaction. Along sidewalks and public spaces, areas should be reserved for artwork, benches, drinking fountains, bike racks, and transit shelters. Street furniture should be located only within the designated sidewalk zones. It should not block, impede or discourage pedestrian activity.

Appropriate:

- Durable, low maintenance materials
- Historically compatible with downtown architecture
- Dark green or black graffiti-resistant paints

Inappropriate:

- Advertising on street furniture
- Synthetic or plastic poles, standards or benches
- Bicycle racks with movable or operable components

Benches



Appropriate: Bench of weather-resistant metal



Appropriate: Wood and iron bench in a historic style



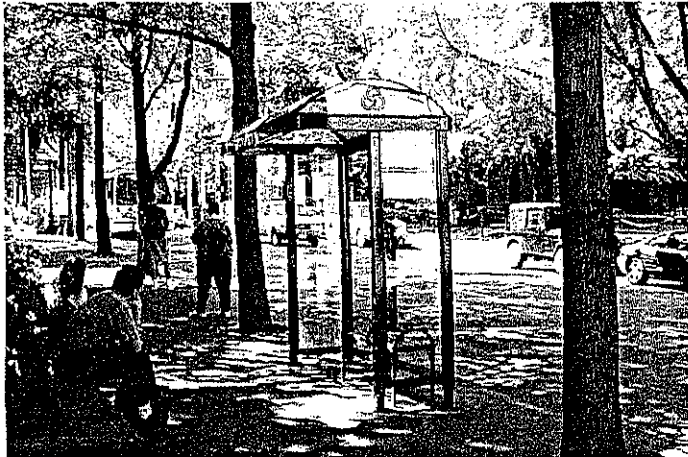
Not Appropriate: Contemporary forms



Not Appropriate: Bench without back

Street Furniture

Transit Stop

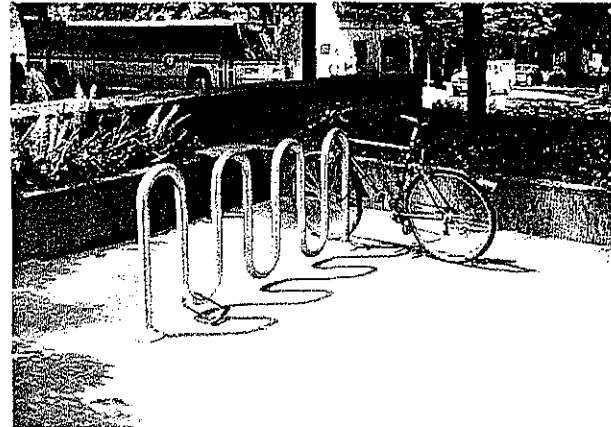


Appropriate: Transit stop with bench and snow, rain, and wind protection



Not Appropriate: Transit stop with no weather protection

Bike Racks

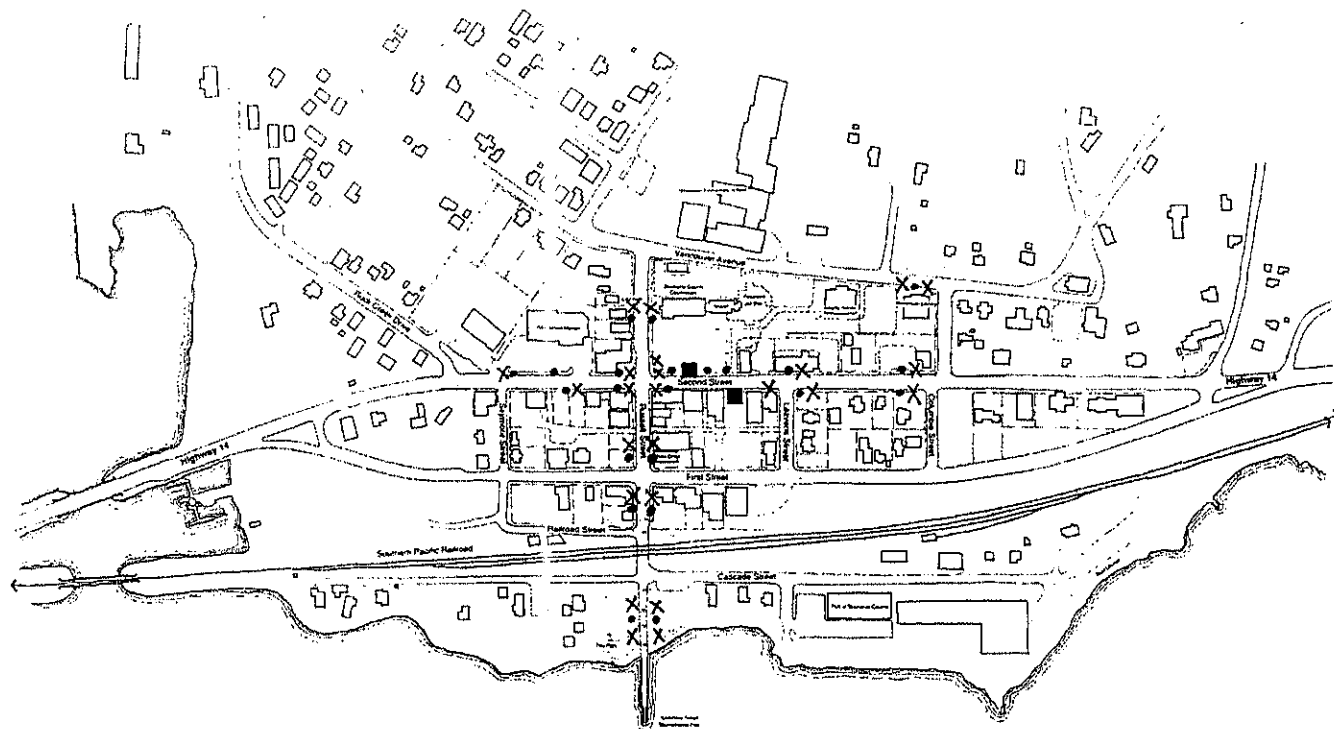


Appropriate: "Ribbon" rack has high capacity and, with space provided



Not Appropriate: Racks with moving parts are often confusing and present a maintenance problem

Street Furniture



Columbia River

- Transit Shelter
- X Bike Rack
- Bench

Railings & Fences

Intent

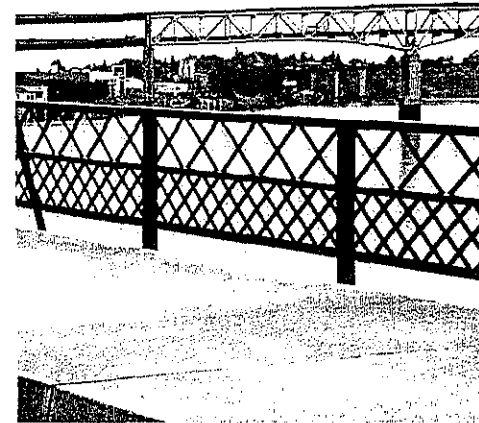
Railings and fences should be used sparingly as design features. They should be used only when required by code or to create or contribute to a visual screen between incompatible uses.

Appropriate:

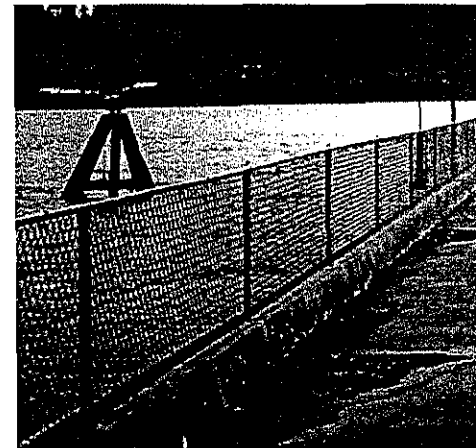
- Ornamental painted (dark green or black) fences
- Wood (with Craftsman architecture only)

Inappropriate:

- Chain-link fence



Appropriate: Lace-like Steel balustrade



Not Appropriate: Chain-link fence on top of concrete curb

Retaining Walls

Intent

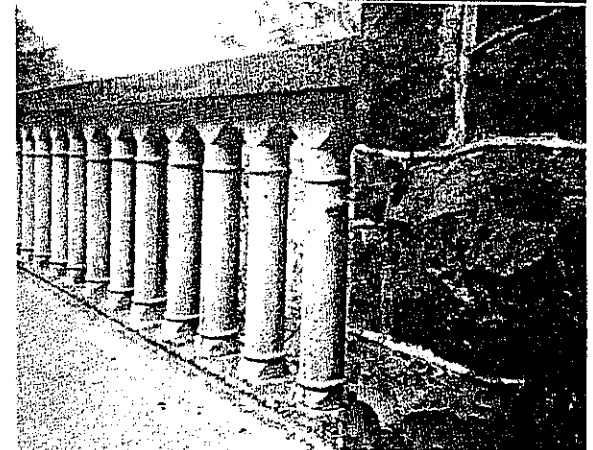
Because of the sloped nature of downtown Stevenson, retaining walls are likely to be required for many developments. The retaining walls should be consistent with the construction technique, materials of the WPA stone work common throughout the Columbia Gorge.

Appropriate:

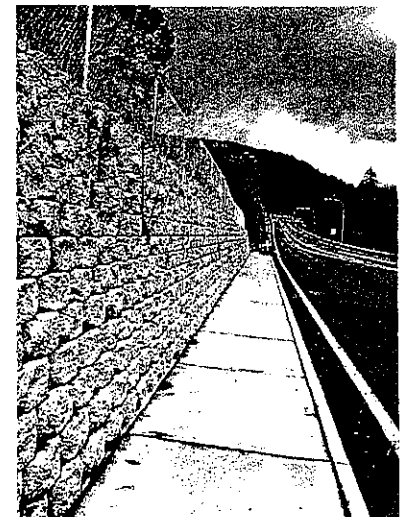
- Indigenous Stone (basalt)
- Integration of concrete pilasters or ballusters
- Integration of light fixtures or wrought-iron fences

Not Appropriate:

- Block retaining wall
- Simulated stamped-stone concrete walls
- Precast concrete wall systems
- Wood ties



Appropriate: indigenous stone retaining walls



Not Appropriate: Keystone block retaining walls

Design Guidelines - Private Development

Purpose

Building Character

Building Form

Windows

Roofs

Walls

Entries

Colors

Sign Concept

Applied Signs

Hanging Signs

Prohibited Signs

Lighting Concept

Exterior Building Lighting

Parking Lot Lighting

Purpose

The private development guidelines ensure that each development site is consistent with the development framework. They express the desired character of future development.

The guidelines recognize and foster maintaining downtown Stevenson as an urban place of buildings, in most instances, built edge to edge and engaging the street. The guidelines promote new construction and rehabilitation that embraces the many unique qualities of the existing urban development.

Buildings should be “good neighbors.” They should be compatible with surrounding buildings by avoiding disruptive excesses. A compatible building should relate to nearby buildings’ scale color, rhythm and proportion.

The Design Guidelines are not retroactive and apply only to new construction, remodeling, and changes of use. For example, an existing business with signs not in compliance with the Design Guidelines would not be required to change the signs at the time the Guidelines become effective. However, if the existing business were to replace the signs after the Design Guidelines come into effect, the new signs would be required to comply. Existing uses and structures are “grandfathered in” in this manner over time.

Architectural Integrity

Development in Downtown Stevenson may involve a great deal of rehabilitation, remodeling or additions to existing structures. The following are minimum standards that should be considered to ensure that the rehabilitation of existing buildings preserves the character downtown.

- Buildings additions should not deform or adversely effect the composition of the facade or be out of scale with the building.
- Historic buildings should be respected and recognized as products of their time. Distinctive, stylistic features such as wood detailing and trim at openings, eaves and cornices, or examples skilled craftsmanship such as quality wood windows and doors should be treated with sensitivity. For example, finely crafted wood windows and doors should either be repaired and reused, or replaced with similar finely crafted substitutes.

Private Development Guideline Goals

Private development decisions must be made with awareness of their urban design consequences. Development should:

- Use quality materials that promote permanence and delight.
- Contribute to a strong organizing pattern. The sum is more important than one individual building.
- Balance functional concerns, efficiency, and cost of construction with the role of the building in the cityscape and the definition of streetspace.

Building Character

The standard promotes architectural elements that unify downtown Stevenson. The architecture should reinforce Stevenson's riverfront development era that occurred generally from 1900 to 1935. The guidelines promote only architectural types that are historically indigenous to Stevenson or the Columbia Gorge area. "Placeless" architectural styles, typical of national chain stores are inappropriate. A small palate of architectural styles is suggested to maintain unity and provide variety.

Appropriate:

- Simple-Unadorned
- Cascadia- Lodge-like
- Craftsman

Not Appropriate:

- Anonymous, slick-skinned contemporary architecture lacking richness and detail
- Placeless, nationwide corporate chain store architecture



Appropriate: Simple unadorned buildings



Not Appropriate: "Placeless" corporate identity

Building Form

Buildings in downtown Stevenson should be consistent and complementary to the fabric of early-20th century buildings that still remain.

Appropriate:

- New buildings of brick and wood are most appropriate, while the use of steel, plastics and concrete on the exterior should be avoided.
- Buildings downtown occupy small sites. New developments that occupy more than a quarter of a block should be broken up to appear as more than one structure.
- Heights of new buildings should not vary more than 25% from the two story height of many existing buildings in the downtown. Structures beyond three stories are incompatible and could overwhelm the scale of downtown.
- Buildings should provide spatial definition for pedestrians along the base of the building. At this street level, buildings should have indentations, projections and rich texture.



Appropriate: Two-story brick building



Appropriate: Two-story, mixed-use wood-panel building



Appropriate: One-story wood frame building



Not Appropriate: Contemporary in form

Windows

Within downtown, only high quality, historically appropriate windows and window materials are suggested. Low quality “reproduction” windows that create a building composition that appears inauthentic or has a “stage-set” appearance are inappropriate. Openings in the exterior walls of buildings add variety and interest at the street level. Business’s prosper as the transparency created at the ground floor invites people in. Restaurants and cafes can benefit even more if they have window that can actually open -- by sliding, pivoting, or shuttering. Windows makes it possible for pedestrians to see, hear, and smell what the business has to offer.

Appropriate:

- Multiple small panes
- Large plate glass (for views)
- Dark wood or metal sashes
- Transparent glazing
- Transom windows
- “Punched”, recessed window openings

Not Appropriate:

- Large plate glass (as primary architectural feature)
- Simulated, divided lites
- Single, small panes
- Windows flush with building wall
- Reflective, opaque or tinted glazing at street level



Appropriate: Operable Plate Glass Windows



Not Appropriate: Small, fixed windows

Roofs

Because Downtown Stevenson is on a hillside, appropriate rooftop treatment is important to avoid adversely impacting surrounding buildings and open spaces.

“Functional Roofs” that are consistent with the suggested architectural styles are appropriate. The roofs in downtown should be calm, consistent, natural and subdued. The roof should not be the primary architectural feature.

Roof mounted mechanical equipment should be hidden from view by parapets. A parapet is a low, protective wall built up along the edge of a roof (an example is at the Rainy Day Gallery and Studio). If building parapets cannot provide adequate screening, mechanical equipment should include walls or enclosures installed as an integral part of the architectural design of the building.

Appropriate:

- 4:12 minimum pitch
- 12:12 Maximum pitch
- Small Dormers - 4:12 Minimum Pitch
- Flat or low pitch screened by parapet walls

Not Appropriate:

- Nonfunctional decorative roofs
- Brightly Colored Roofs (red, green, blue, tan colors)
- Mansard or other applied decorative roof forms

Materials:

- Cedar Shingle
- Three Tab
- Metal (corrugated or small folded, standing seam)
- Muted Dark Colors- grays, blacks, dark green, dark brown



Appropriate: Dark metal, gable ended roof



Appropriate: Small dormers



Not Appropriate: Mansard or other applied decorative roofs

Walls

Historic or authentic-appearing exterior cladding materials should be used. Wall materials and application techniques that are consistent with the building character should be fostered. Long lasting materials that are durable and require little maintenance should be employed.

Appropriate:

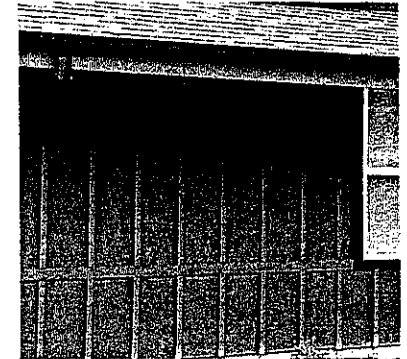
- Smooth Bevel Drop Siding
- Shingles
- Vertical Siding (Wood Board & Batten)
- Brick
- Native (Basalt) Stone Masonry

Not Appropriate:

- Metal Panels
- Wood Shakes
- Exposed Concrete Block
- Flagstone or Other Applied Stone Veneer Products
- Large Precast Concrete Panels
- Cast-in-place Concrete
- Plywood Panelling (T-111)
- Synthetic Cladding Materials at First Floor (Stucco/ Fiberglass Panels)



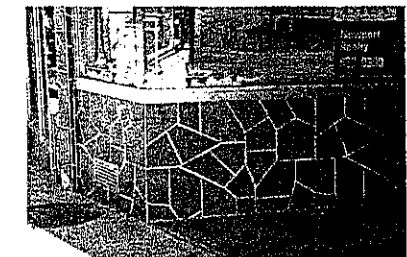
Appropriate: Cedar shingles



Appropriate: Board and Batten



Appropriate: Native stone (basalt) masonry



Not Appropriate: Flagstone Veneer

Entries

Doorways should be located so that buildings appear welcoming and inviting. Corner entries and recessed doorways are suggested. Retail shops and businesses should have individual entries from the street rather than from interior spaces such as lobbies or atriums.

Appropriate:

- Wood
- Recessed
- Transparent with single or multiple lites
- Corner

Not Appropriate:

- Large glass doors
- Security gates
- Solid wood or metal doors
- Overhead garage doors
- Aluminum frame
- Entryway plazas or forecourts



Appropriate: Recessed doorway



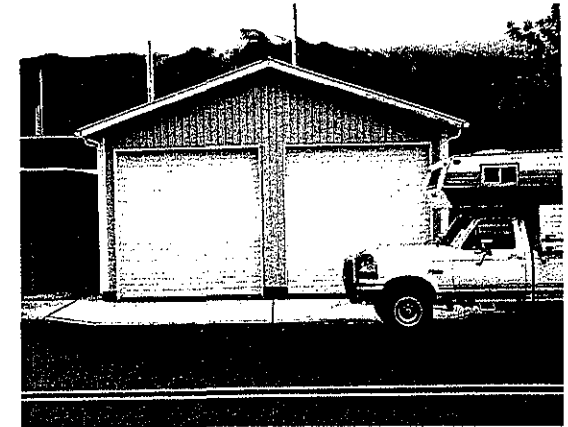
Appropriate: Wooden door frame



Appropriate: Wooden door frame with wood detailing above



Not Appropriate: Thin, aluminum frame in narrow recess



Not Appropriate: Overhead Garage doors engaging public streets

Colors

Only building colors that are consistent with the architectural character or suggested palate of architectural styles should be used.

Appropriate:

- Shades of white
- Natural and “weather accelerating”
- Dark color doors, windows and trim

Not Appropriate:

- Bright primary colors
- Untreated wood



Appropriate: Shades of white



Not Appropriate: Bright blue

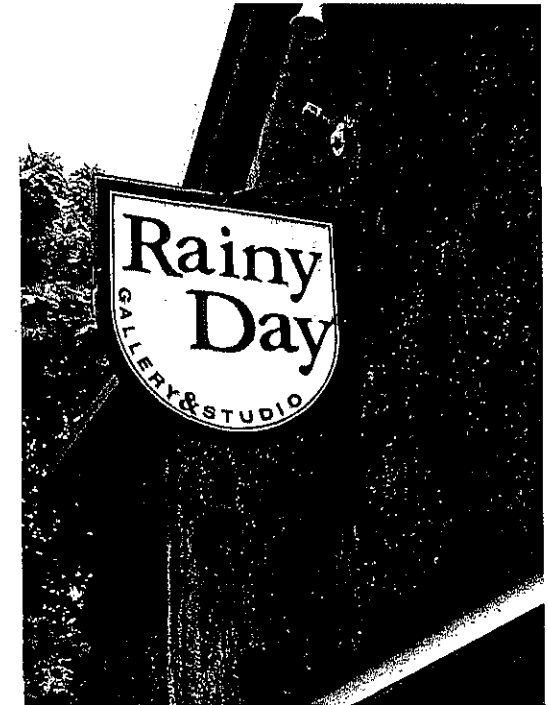
Sign Concept

The guidelines identified herein supercede existing ordinances. In instances where no guideline is provided, sign proposals must meet the standards and requirements of the Stevenson's Zoning Ordinance. Advertising, construction, political or portable signs must be consistent with the City's Zoning Ordinance regulations. These guidelines apply to permanent building, tenant or informational and temporary signs only.

Public events are best advertised on banners affixed to street light standards. Design standards for banners should be established. Applications and concise plans for banners, setting forth dimensional requirements, materials of construction, mounting heights, color and graphic format shall be subject to review and approval. An additional means of advertising public events and information would be to post approved material at a kiosk located in the proposed courthouse plaza.

Signs may provide an address, identify a place of business, locate tenants, reinforce the downtown's character or generally provide directions and information. Regardless of function, signs should be architecturally compatible and contribute to the character of the area. Signs should not compete with each other or dominate the setting due to inconsistent height, size, shape, number, color, lighting or movement.

Signs should not overwhelm the building or the landscape. They should instead provide easily recognizable business identification and information in a highly graphic format that is complementary to the Downtown Stevenson's architecture. Tasteful logos, symbols and graphics are encouraged. A strong pedestrian orientation is encouraged.



Signs should be highly graphic and oriented toward the pedestrian

Each development or building represents only a small portion of the downtown as a whole, but contributes significantly to the overall visual image of downtown. The uniform application of sign guidelines addressing type, location, size and quality will ensure a visually pleasing downtown environment.

Applied Signs

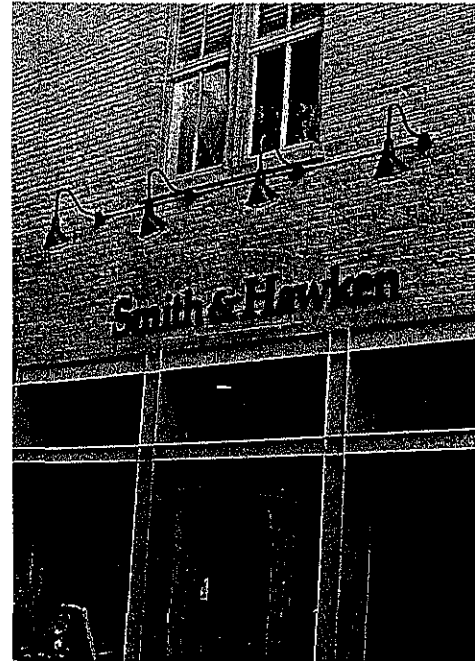
Applied wall signs can be located along the top, middle or at the pedestrian level of buildings. Signs can be incorporated into the building architecture as embossing or low-relief casting. They may be painted, or applied metal lettering and graphics. Signs should be durable and long lasting. They may incorporate lighting as part of their design (see guidelines in Lighting). Signs should identify the building or business's name and should not be used to advertise products or services.

Appropriate:

- Front-lit
- Historic in character
- Modest scale

Not Appropriate:

- Plastic
- Backlit of any material



Appropriate: Front lit metal wall signs



Not Appropriate: Internally lit plastic sign

Hanging Signs

Hanging signs should be very graphic and constructed of high-quality materials and finishes. They should be attached to the building with durability in mind. Signs should be compatible and complement the building's architecture, awnings, canopies, lighting, and street furniture.

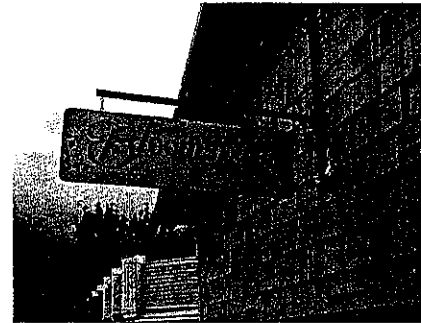
Hanging signs should be highly visible from the sidewalk. Generally, they should be attached perpendicular from the building, or at 45 degree angles from corners.

Appropriate:

- Small Scale (9 sq. ft. recommended, maximum 15 sq. ft.)
- Historic in character
- Front lit

Not Appropriate:

- Backlit of any material



Appropriate: Graphic hanging sign



Not Appropriate: Difficult to read hanging sign

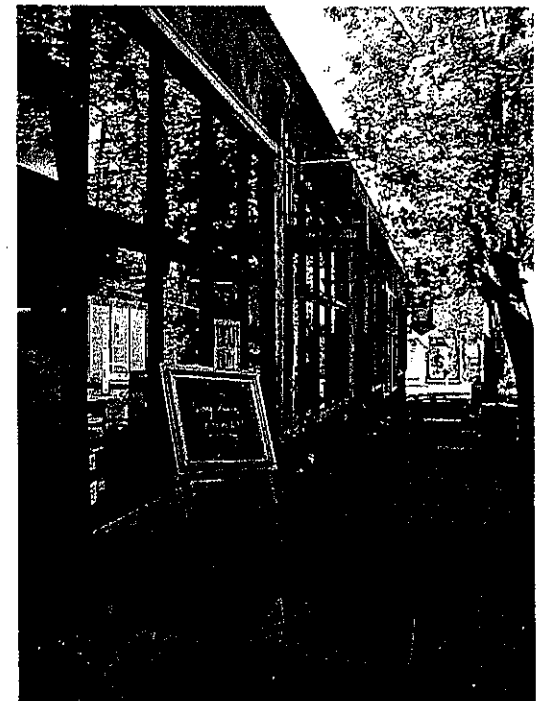
Temporary Signs

Signs identifying uses or activities temporary in nature, such as “sandwich board” signs or signs associated with real estate sales and leasing and the development and construction of buildings, shall be allowed on a temporary basis if consistent with the design character of the surrounding area.

Temporary signs must be located on or in close proximity to the uses identified.

The large number of signs often associated with development, design, construction and leasing shall be combined into one sign for each project and the sign shall be located on site.

Applications and concise plans for temporary signage, setting forth dimensional requirements, materials of construction, mounting heights, color and graphic format shall be subject to review and approval.



Appropriate: Temporary sign consistent with the design character of the surrounding area

Prohibited Signs

- Exterior neon
- Monument signs
- Freestanding pole signs and roster boards
- Moving or animated signs that create traffic hazards
- Obsolete or abandoned signs (remove within 30 days of business closure)
- Temporary signs on public property including signs on traffic signals, utility poles, trees, etc.
- Vehicle or portable signs
- Off-premise signs
- Rooftop signs
- Inflatable or balloon signs
- Magnetic signs
- Painted window advertising signs
- Plastic flags
- Signs in poor repair



Not Appropriate: Moving or animated signs

Lighting Concept

Lighting should not only provide nighttime security, but also encourage nighttime patronage of businesses and restaurants. Lighting of special elements should create an atmosphere of festivity and activity. Utilitarian applications of glaring, or offensively-colored lights should be prohibited.

Parking lot, building, and pedestrian lights play a vital role in defining a quality appearance within downtown. The lighting concept has been devised to provide a hierarchy of lighting effects that will contribute to an overall unified downtown image.



Exterior Building Lighting

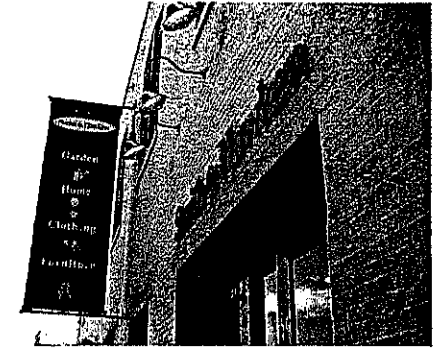
Architectural lighting should be integrated with building elements and landscaping. A limited number of lights may be used to create shadow, relief, and outline effects. Lighting should be used for wall washing, wall highlighting, sign illuminating, and be directed towards the building, not the public right-of-way. Lighting should not cast any glare onto adjacent lots in such a manner as to decrease the safety of the pedestrian, vehicular movement or effect the livability of nearby residents or businesses.

Appropriate:

- Decorative, historic wall sconce and similar architectural lighting fixtures.
- Metal Halide or other natural color spectrum lamp
- “Gooseneck” or similar architectural lighting fixture
- Integrated and screened uplights or spotlights

Not Appropriate:

- Sodium
- Neon
- Fluorescent Tube
- Broadcast Lighting
- Flashing Lighting



Appropriate: “Goose neck” lighting



Appropriate: “Goose neck” lighting

Parking Lot Lighting

Parking lot lighting should clearly define accessways to streets and building entries. It should be directed downward so that it does not produce glare into surrounding areas. Historic ornamental fixtures that are in character with downtown architecture and are compatible with the street furniture should be used. Pole standards should be black or very dark green color. Parking lot lights should be integrated into landscape islands. Building security lights (flood-lights) should not be used as substitutes for parking lot lighting. The lighting should be bright enough to provide a sense of security and safety.

Appropriate:

- Historic fixtures
- Pedestrian scale
- Dark pole standards

Inappropriate:

- Cobra-head type fixtures
- Contemporary fixtures
- "Broadcast" security lighting
- Motion-detecting security lighting
- Lighting recessed in bollards or other lighting at low heights

Development Standards

Background

Key Distinctions: Development Framework & Standards
and Existing Code

Existing Land Use Zones and Proposed
Development Zones

Development Standards Matrix

Development Standards Matrix Notes

Applying the Development Standards

Background

Downtown Stevenson is divided into two development zones, Zone 1 and Zone 2. Located within Zone 1 are the two primary retail streets that hold the key to success for the downtown. Adherence to the guidelines within Zone 1 is paramount for this is where the majority of pedestrian activity will occur. Zone 2, on the other hand, allows more flexibility for uses and does not include the primary active streets in the downtown.

Zone 1 is designated as reaching 100 feet deep on each side along Second Street from Rock Creek Drive to Columbia Street and Russell Street from Vancouver Avenue to Railroad Street. Zone 2 includes the area between the confluence of First and Second Street at their east and west ends, Vancouver Avenue to the north and the Columbia River to the south.

The Development Standards supercede the existing Development Code where discrepancies exist. They are regulatory requirements which are not negotiable during the Design Review Process.

The Development Standards are not retroactive and apply only to new construction, remodeling, and changes of use. For example, an existing use with on-site parking not in compliance with the Development Standards would not be required to provide the required number of spaces at the time the Development Standards become law. However, if the existing use were to change, the new use and related parking provided would both have to be in compliance with the Development Standards. Existing uses and structures are "grandfathered in" in this manner over time.

Key Distinctions: Development Framework & Standards and Existing Code

Key distinctions between the proposed Development Framework and Standards and existing code are as follows:

Zone 1 Land Uses

Principal uses no longer permitted in Zone 1 (previously zoned C1):

- Single-family detached dwelling
- Bank, financial institution
- Residential care facility
- Conditional uses are no longer permitted in Zone 1.

Zone 2 Land Uses

Principal uses no longer permitted in Zone 2 (previously zoned C1):

- Single-family detached dwelling
 - Residential care facility
- Previous conditional uses permitted as principal uses:*
- Parking structure
 - Vehicle repair
 - Park and playground
 - Church
 - Library
 - Child Care Center
 - Food or beverage operation with no drive-through service
 - Government administration building

Conditional uses no longer permitted in Zone 2 are:

- Hazardous waste storage
- Communication tower in excess of fifty feet high
- Temporary emergency, construction or repair residence
- Light industrial activities

Active Street Frontage

The requirement for 50% glazing at groundfloor street frontages along designated active streets exceeds ground level wall area requirements in the C1 Zone District. No substitutions are allowed in Zone 1.

Lot Size/Density, Yard and Lot Coverage Requirements

Lot Size/Density, Yard and Lot Coverage requirements are superceded by the Development Standards for properties within development zones 1 and 2. Minimum lot area, width and depth requirements are eliminated except for multifamily and apartment uses. Maximum building coverage for multifamily and apartment uses is greater than that allowed in the existing code. Maximum front yard setbacks are reduced to 0' in Zone 1 except for multifamily and apartment uses.

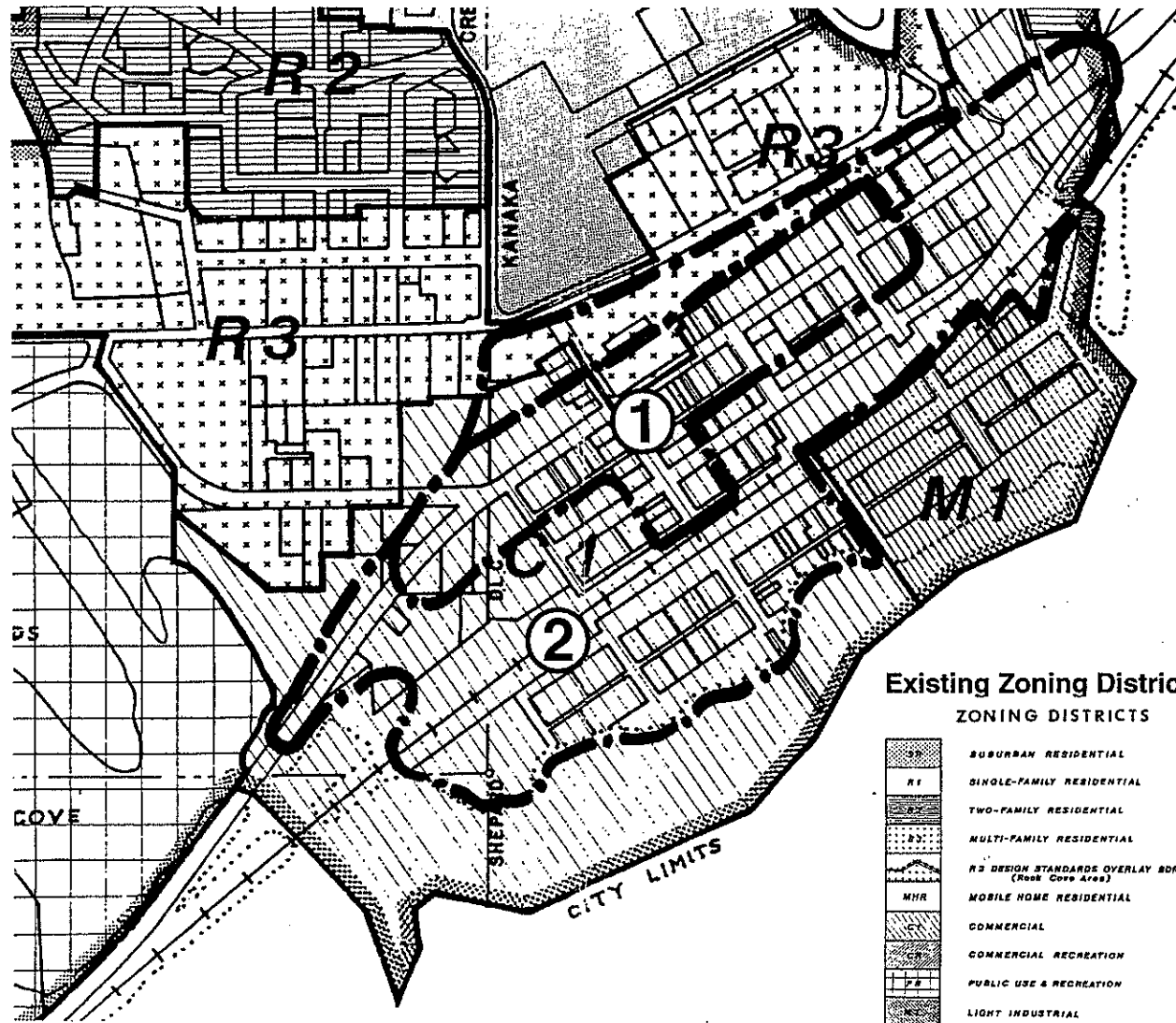
Parking

Parking requirements are 3 spaces per 1000 SF building floor area. For most uses, this is lower than what was previously required. In lieu of the parking requirement, property owners may choose to pay an equivalent systems development charge (to be determined) for the required spaces off-site. Requirements for multifamily and apartment uses are lowered to 1 space per unit.

Signs

Guidelines identified in Design Guidelines - Private Development (pages PD-9 through PD-12) supercede existing ordinances.

Existing Land Use Zones and Proposed Development Zones



Existing Zoning Districts

ZONING DISTRICTS	
R2	SUBURBAN RESIDENTIAL
R1	SINGLE-FAMILY RESIDENTIAL
R2	TWO-FAMILY RESIDENTIAL
R3	MULTI-FAMILY RESIDENTIAL
R3 DS	R3 DESIGN STANDARDS OVERLAY AREA (Rock Cove Area)
MHR	MOBILE HOME RESIDENTIAL
C	COMMERCIAL
CR	COMMERCIAL RECREATION
PR	PUBLIC USE & RECREATION
LI	LIGHT INDUSTRIAL

Proposed Development Zones

- Zone 1 - Primary Retail Corridor
- Zone 2 - Supporting Commercial and Service Uses

Development Standards Matrix

"-" indicates no requirement.

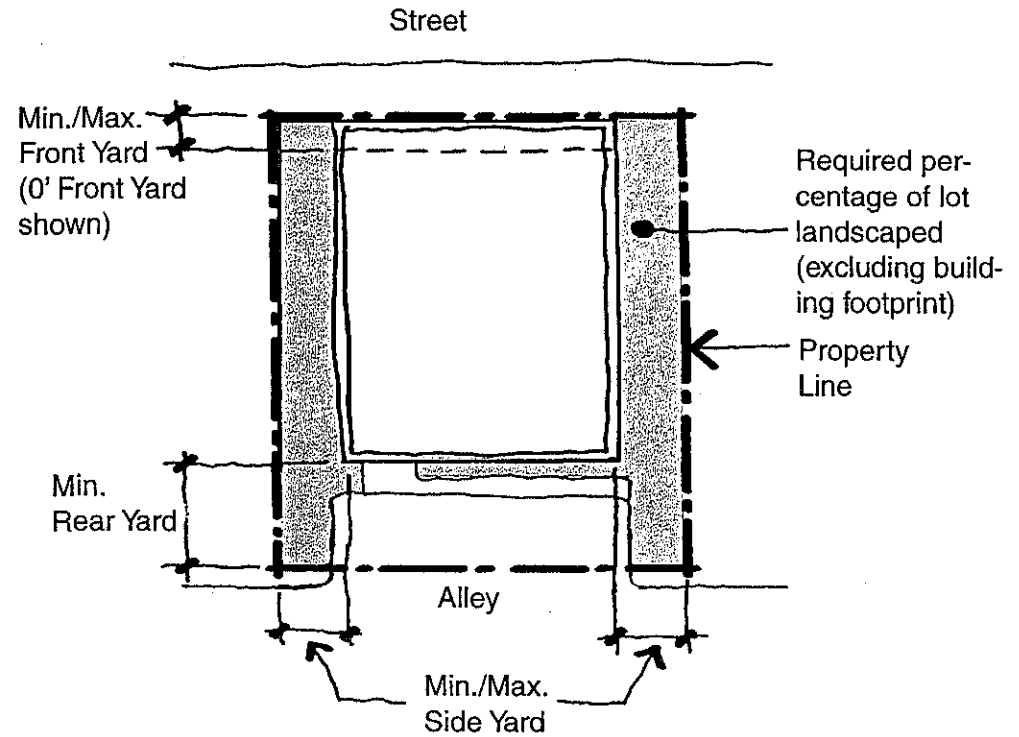
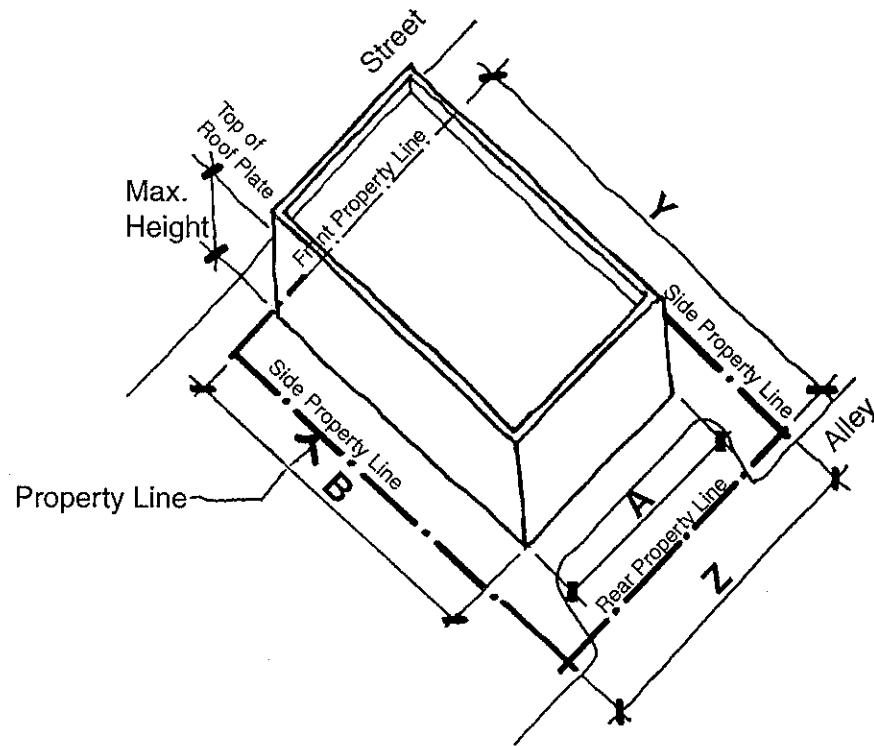
Notes located on following page.

Development Zone	Land Use	Lot Size /Density				Yard			Lot Coverage Max. %	Landscape Min. % (Previous Area Excluding Lot Coverage)	Building Height Max Stories/ft. (To Top of Roof Plate)	Parking Number Spaces Required per 1000 SF floor area Note 17	
		Principal Use	Min. Lot Area	Min. Lot Width	Min. Lot Depth	Units/Acre (Max)	Min./Max. Front Yard Note 13	Min./Max. Side Note 14					Min. Rear Yard Note 15
Zone 1 Primary Retail Corridor	Retail Sales - Note 2	-	-	-	-	0'/0'	0'/10'	0'	1	0'	3 stories/max. 50'	3	
	Office - Note 3	-	-	-	-	0'/0'	0'/0'	0'	1	0'	3 stories/max. 50'	3	
	MF Dwelling and Apartment - Note 3	1200 SFA/n.	-	-	-	24	0'/15'	0'/0'	0'	80	10% - Note 16	3 stories/max. 35'	1 space/unit
	Food/Beverage Operation - Note 4	-	-	-	-	-	0'/0'	0'/0'	0'	1	0'	3 stories/max. 50'	3
	Hotel - Note 5	-	-	-	-	-	0'/0'	0'/0'	0'	1	0'	3 stories/max. 50'	3
	Theater	-	-	-	-	-	0'/0'	0'/0'	0'	100%	0'	3 stories/max. 50'	3
No Conditional Uses Permitted													
Zone 2 Supporting Commercial and Service Uses	Principal Use												
	Retail Sales - Note 6	-	-	-	-	0/-	0'/10'	0'	100%	10%	3 stories/max. 50'	3	
	Office	-	-	-	-	0/-	0'/10'	0'	100%	10%	3 stories/max. 50'	3	
	MF Dwelling and Apartment	1200 SFA/n.	-	-	-	24	0'/15'	0'/0'	0'	80%	10% - Note 16	3 stories/max. 35'	1 space/unit
	Food/Beverage Operation - Note 4	-	-	-	-	-	0'/-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Hotel - Note 5	-	-	-	-	-	0'/-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Theater	-	-	-	-	-	0'/-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Bank	-	-	-	-	-	0'/-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Parking Structure - Note 18	-	-	-	-	-	0'/-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Park and Playground	-	-	-	-	-	0'/-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Church	-	-	-	-	-	0'/-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Child Day Care Center	-	-	-	-	-	0'/-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Library	-	-	-	-	-	0'/-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Government Administration Building	-	-	-	-	-	0'/-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Conditional Use												
	Automobile Service Station - Note 7	-	-	-	-	-	0'/-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Parking Lot	-	-	-	-	-	10'/- Note 19	5'/- Note 19	5' Note 19	100%	20%	3 stories/max. 50'	3
	Vehicle Repair - Note 8	-	-	-	-	-	0'/-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Fire, Police & Emergency Services Station	-	-	-	-	-	0'/-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
	Hospital, Nursing Home or Convalescent Center	-	-	-	-	-	0'/-	0'/10'	0'	100%	10%	3 stories/max. 50'	3
Utility & Communications Facilities - Note 10	-	-	-	-	-	0'/-	0'/10'	0'	100%	10%	3 stories/max. 50'	3	
Railroad Facilities - Note 11	-	-	-	-	-	0'/-	0'/10'	0'	100%	10%	3 stories/max. 50'	3	
Piers and Docks, Watercraft	-	-	-	-	-	-	-	-	100%	10%	3 stories/max. 50'	3	

Development Standards Matrix - Notes

- 1) Permitted as a primary use in Zone 1 and Zone 2: Canopy, marquee or awning extending into a public right-of-way.
- 2) No services permitted at street level.
- 3) Permitted at upper stories only.
- 4) No drive-through services permitted.
- 5) No motel, bed and breakfast operation, tourist home, and boarding or rooming house permitted.
- 6) Includes personal and other services, and rental operations.
- 7) Permitted provided that: 1) no major repair work is undertaken, 2) vehicles are not parked or stored overnight unless inside a fully enclosed building and 3) truck, trailer or equipment rental operations are conducted only inside a fully enclosed building.
- 8) Permitted provided that: 1) no repair work is done outside of a fully enclosed building; and that 2) vehicles are not parked or stored overnight unless inside a fully enclosed building.
- 9) Permitted provided the activity is an integral part of an on-premises principal use.
- 10) Permitted to serve the surrounding area; but exempting from the conditional use process underground cables, pipelines, vaults, and the like, and overhead cables supported on poles not over fifty feet in height.
- 11) Includes a ticket office/waiting room.
- 12) Conditional uses permitted in Zone 2 include any other use determined to be of the same general character as the principal uses or conditional uses permitted outright.
- 13) Minimum front yard setback at shoreline: 50'
- 14) 5' minimum and 0' maximum side yard setback for corner lot abutting an R District.
- 15) 20' minimum rear yard setback for 3 story building when abutting an R District; 10' minimum rear yard setback for 2 story building abutting an R District.
- 16) Balconies, terraces and roof gardens can be counted as open space.
- 17) In lieu of the parking requirement, property owners may choose to pay an equivalent systems development charge (to be determined) for the required spaces off-site.
- 18) Required active ground floor use along the street.
- 19) Required minimum setbacks to be landscaped.
- 20) R3 Zone District within proposed development zones to be Government Administration Building use only.

Applying the Development Standards



Determining Lot Size/Density, Lot Coverage & Building Height

$$\begin{aligned} \text{Building Footprint} &= A \times B \\ \text{Lot Area} &= Y \times Z \end{aligned}$$

$$\text{Lot Coverage} = \frac{\text{Building Footprint}}{\text{Lot Area}}$$

Determining Landscape and Yard

PROJECT:

STEVENSON DEVELOPMENT REVIEW CHECKLIST

	Does Apply	Does Not Apply	Does Comply	Does Not Comply
DEVELOPMENT STANDARDS				
▪ Principal Use/Conditional Use	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Lot Size/Density	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Yard	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Lot Coverage	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Landscape	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Building Height	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Parking	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
DEVELOPMENT FRAMEWORK				
▪ Build-To Line	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Active Street Frontage	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Rain Protection	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
DESIGN GUIDELINES				
Public Areas				
▪ Landscaping	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Street Lighting	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Sidewalks	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Curb Extensions	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Pedestrian Street Crossings	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Street Furniture	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Railings & Fences	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Retaining Walls	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Private Development				
▪ Active Street Frontage	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Build To Line	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Rain Protection	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Building Character	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Building Form	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Windows	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Roofs	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Walls	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Entries	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Colors	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Applied Signs	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Hanging Signs	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Exterior Building Lighting	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
▪ Parking Lot Lighting	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>