



# City of Stevenson

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Phone (509)427-5970  
FAX (509) 427-8202

7121 E Loop Road, PO Box 371  
Stevenson, Washington 98648

October 8, 2021

Michael Williams  
SW Region Local Programs Engineer  
11018 NE 51<sup>st</sup> Circle  
Vancouver, WA 98682-6686

**Re: 1st Street Pedestrian Amenities and Overlook Project**

Dear Michael,

This letter is to summarize the history and issues of the City of Stevenson's 1st Street Pedestrian Amenities and Overlook project (attached exhibit A) and to work together on a path forward. Our goal is to reach an agreement which will allow the City to construct this project and ensure the long-term infrastructure needs of the residents are met.

## **Executive Summary**

The City was awarded a grant in 2019 to construct vegetated curb extensions, new sidewalk, crosswalk striping, a pedestrian overlook, connection to an existing waterfront path, and street scaping. This will slow down traffic, extend walkability for our downtown core, improve safety for pedestrians and allow a safe alternative access to the City's waterfront. The project has stalled when it was discovered the WSDOT, not the City owned the right of way. A turn-back agreement would be required between the City and WSDOT to construct the project as described in the grant proposal and agreements. The timeline to finalize a turn-back agreement would not be in line with the grant requirements. The options posed to the City are:

- 1) Construct an additional \$300k worth of street improvements to meet DOT standards, which may be torn out when another street is moved in the next 5-years;
- 2) Accept a turn-back agreement to take over the right of way on a road which is in substandard condition; only maintenance projects can take place and the WSDOT budget is stretched thin; or
- 3) Shelve the project until the turn-back issue can be addressed and risk having to refund the \$133k in reimbursements already received for design if the project cannot move to construction by January 2030.
- 4) Abandon the project sooner than January 2030 and refund the \$133k grant reimbursements.

## History

In 1996, WSDOT and the City of Stevenson partnered on the Stevenson Two-Way Couplet project. This involved acquisition of rights of way to extend First Street from Seymour Street west and from Columbia Avenue east to connect to Second Street/SR 14. WSDOT acquired the rights of way and at the end of the project only turned back portions of slope and longitudinal easements along First Street to the City of Stevenson (attached exhibit B).

City and WSDOT staff have changed through the 25 years since the project was completed, and most of the history regarding the ownership of the rights of way was lost. The City has been maintaining all of First Street, including repairing potholes, plowing, etc., for at least the past 15 years. Improvements such as the 1<sup>st</sup> Street Pedestrian Amenities project have been programmed by the City, RTC, and WSDOT. Additional improvements had been conceptualized by the City, including reconstructing the failing roadway along First Street from Columbia Avenue to Second Street.

## Issue

In March 2021, around the time plans reached 75% design, we were notified by WSDOT that the City did not own the right of way and to move forward the project would need to either comply with DOT standards or the City would need to take over the right of way through a turn-back agreement. Since then, the City has been working with WSDOT to understand the DOT requirements as it pertains to this project and evaluate the conditions under which a turn-back agreement would be accepted.

The City is unable to move forward with the project and comply with DOT standards at this time. A feasibility study is beginning to look at the shifting of Columbia Avenue 40' to the east (attached exhibit C). DOT standards would require stormwater upgrades at the intersection of Columbia Avenue and First Street which the City is not willing to spend time or money on until the results of the study are completed. The same goes for the requirement of a traffic study at the same intersection. A rough estimate for the cost to comply with the entirety of the standards is \$300k.

In conversations with past administration, the Stevenson Two-Way Couplet Project strained the relationship between WSDOT and the City in part because the Stevenson community demanded a two-way couplet design instead of the WSDOT engineers' preferred one-way couplet. There were concerns about the quality of construction, some of which were outlined in a letter from the City Administrator Mary Ann Duncan-Cole to the Construction Project Engineer Jim McClellan on October 8, 1998 (attached exhibit D). Specifically, Mary Ann stated concerns over pavement settlement near the retaining wall. Jim's response in a letter dated September 25, 1998 (attached exhibit E) were that the settlement would continue to be monitored to see how it behaves over time. There is longitudinal cracking over ½ inch in this area and visible settling. A condition of the City accepting a turn back agreement would be to address the cause of the settlement and repair of the road. The current financial condition of WSDOT only allows for routine maintenance and the City understands the timeframe for a long-term solution would not meet the grant deadlines, again due to WSDOT budget and prioritization constraints.

## Goals

The end goal for the City is to have a safe First Street corridor for all modes of transportation, provide an overlook for pedestrians, improve walkability of the downtown core, and to take over the ownership of the right of way. This all hinges on whether WSDOT can repair the deficiencies identified in 1998 in time for the City to accept the property transfer and move to construct the 1<sup>st</sup> Street project prior to the grant deadlines expiring.

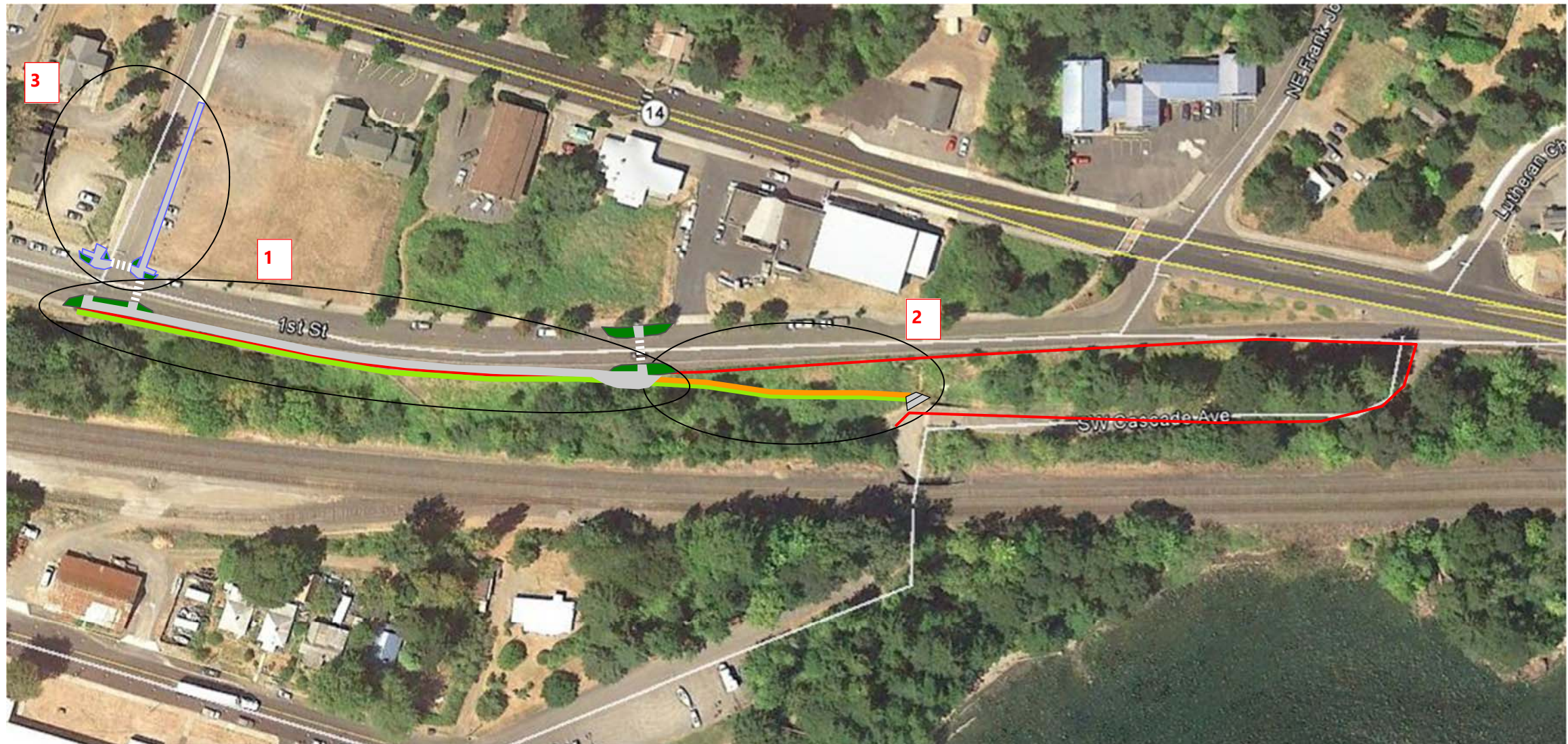
The City requests assistance in developing a way forward for resolving the settlement of First Street to enable the approval of a turn back agreement, or to outline an acceptable deviation process from DOT standards, in order for the City to construct the 1st Street Pedestrian Amenities and Overlook project within the grant deadlines.

Thank you in advance for your efforts in this regard. Please do not hesitate to contact me.

Sincerely,



Leana Kinley, City Administrator  
[leana@ci.stevenson.wa.us](mailto:leana@ci.stevenson.wa.us)



**Overall Project Benefits**

- Calms east and westbound traffic on 1st Street where speeds exceed posted limit.
- Shortens walking distance route from 1,700' (red) to 750' (green) and avoids highway danger.
- Makes key east downtown investment.

**#1 TAP Project Overview**

- Fill ~600 ft gap in pedestrian network with new sidewalk.
- Construct pedestrian overlook with views of Columbia Gorge.
- Install new traffic-calming vegetated curb extensions, and storm water controls as necessary.

**#1 Key Pedestrian Enhancements**

- Narrows crosswalk distances.
- Creates walkable loop from waterfront to downtown.
- Adds street trees, landscaping, way-finding stones, and overlook.

**#2 Footpath to Waterfront (Existing)**

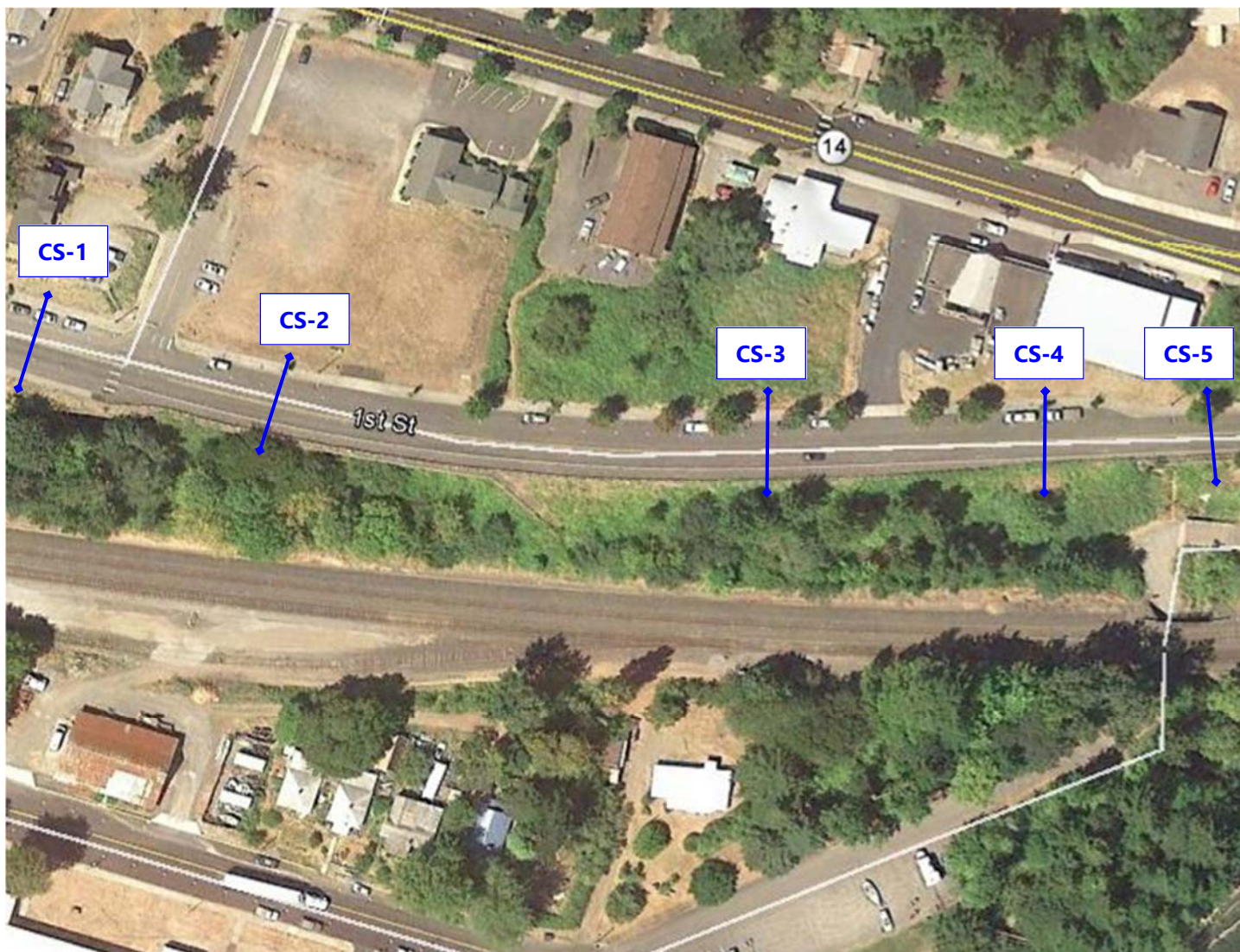
- Installed in 2018 through partnership with PCTA & WTA volunteers.
- ~2'-wide compacted gravel pathway
- Contains 2 stone steps at eastern end.
- Separated from 1st Street by guardrail
- Consider paving at future date, TBD.

**#3 Columbia Avenue (Future)**

- Complete gap in east-sidewalk
- Add vegetated curb extensions and crosswalks across Columbia
- Fund by requiring improvements as part of future development impacting this intersection, STP, or TIB

# 1st Street Traffic Calming, Sidewalk, Overlook Project

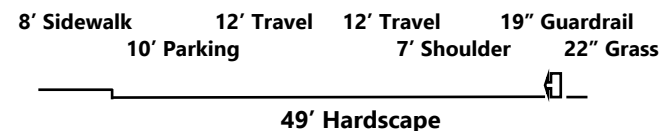
## 2019 Transportation Alternatives Grant Proposal



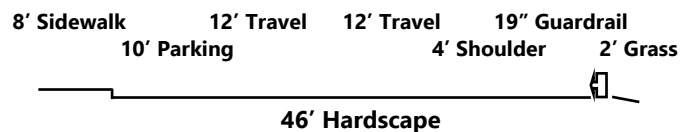
### Cross Section 1 - North—>South



### Cross Section 2 - North—>South



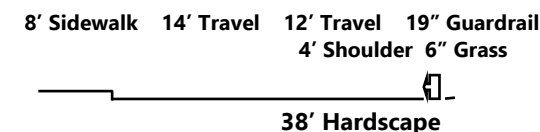
### Cross Section 3 - North—>South



### Cross Section 4 - North—>South



### Cross Section 5 - North—>South



### Overall Project Benefits

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# Waterfront Pathway Connection Project

## Phase 1-PCTA Construction 2018

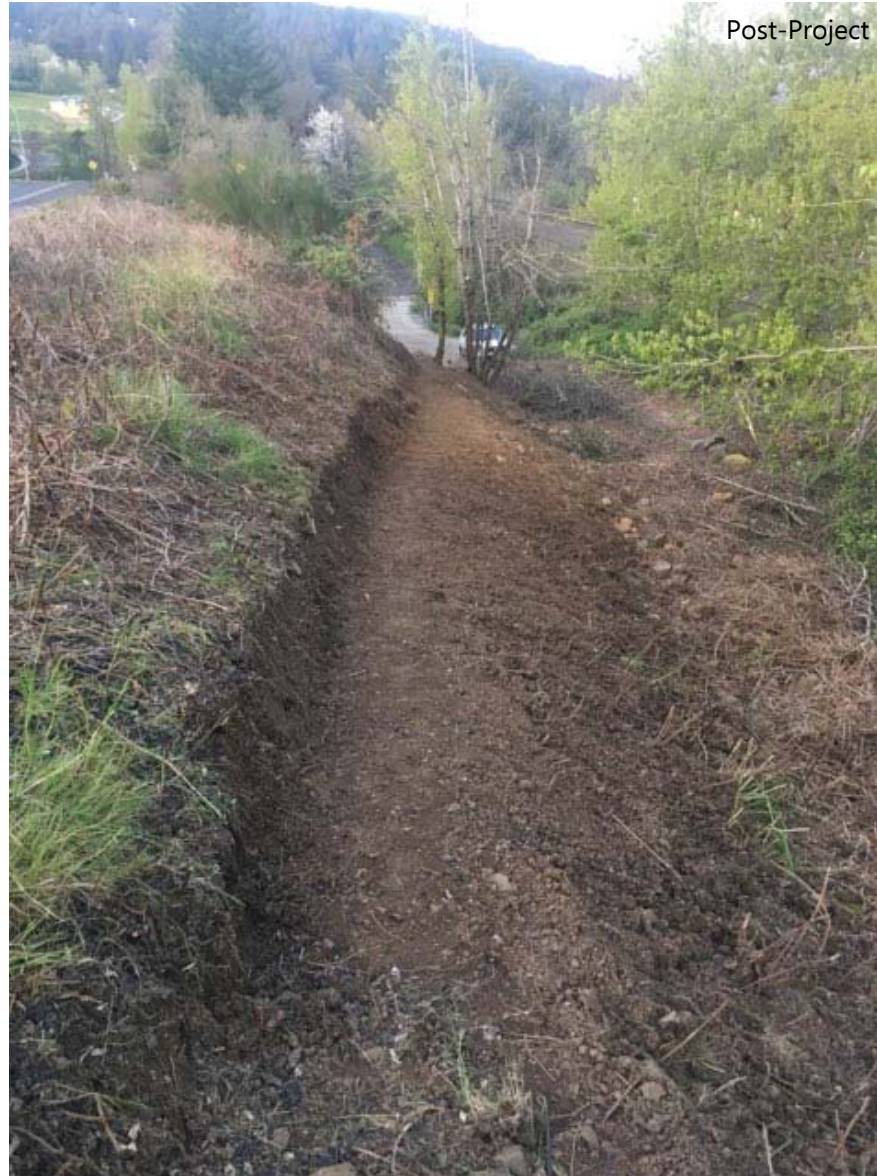
Pre-Project



Press



Post-Project



Post-Project

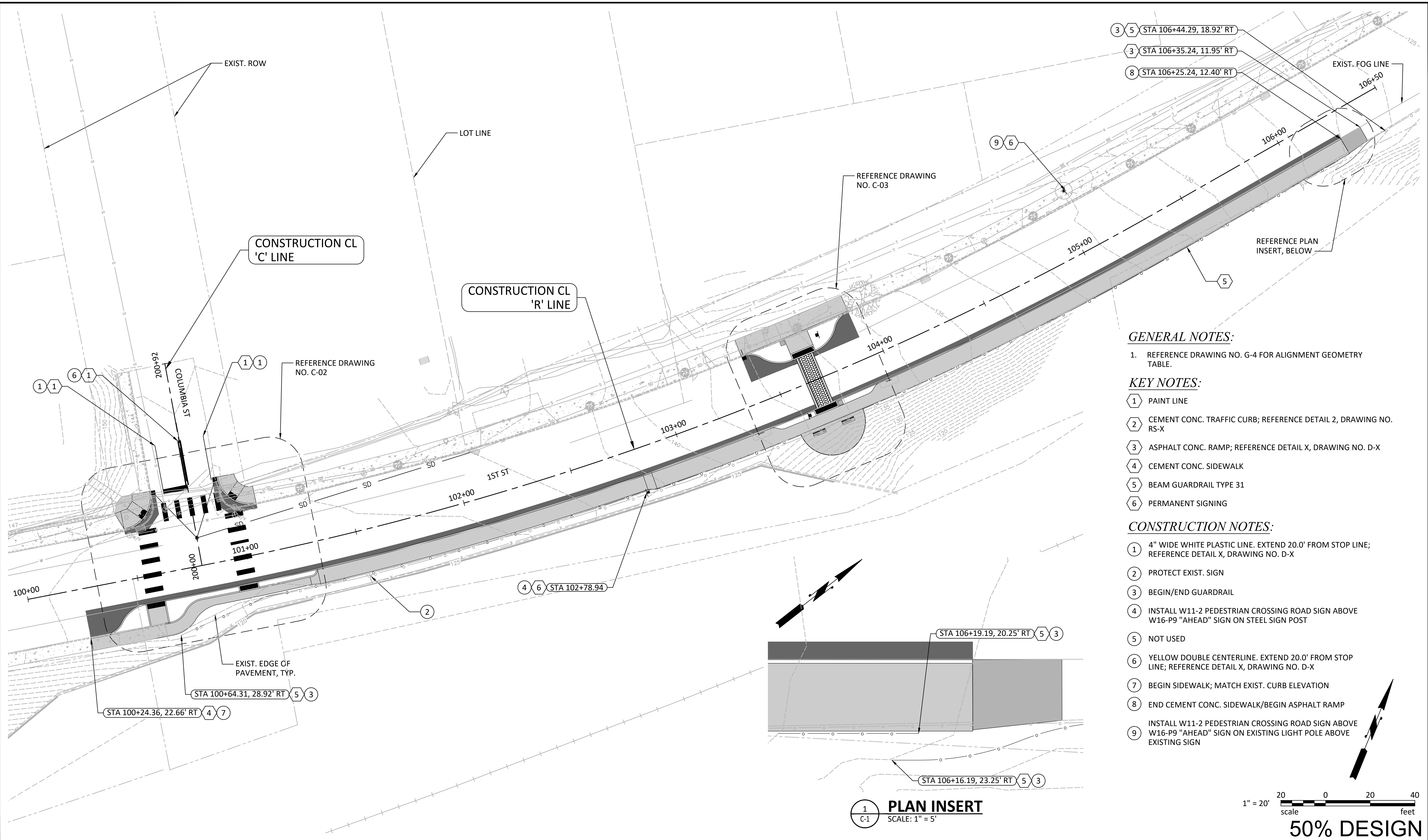


### Phase 1 Project Description

- Partnership with PCTA & WTA
- ~200' long x 2' wide gravel tread path-way
- ~15% slope
- 2 small stairs near Kanaka Creek

### Phase 2 Project Needs

- Guardrail modification at top of path-way
- ~500' sidewalk and/or traffic calming from pathway to Leavens Street
- 1 or 2 crosswalks
- Stormwater controls



**GENERAL NOTES:**

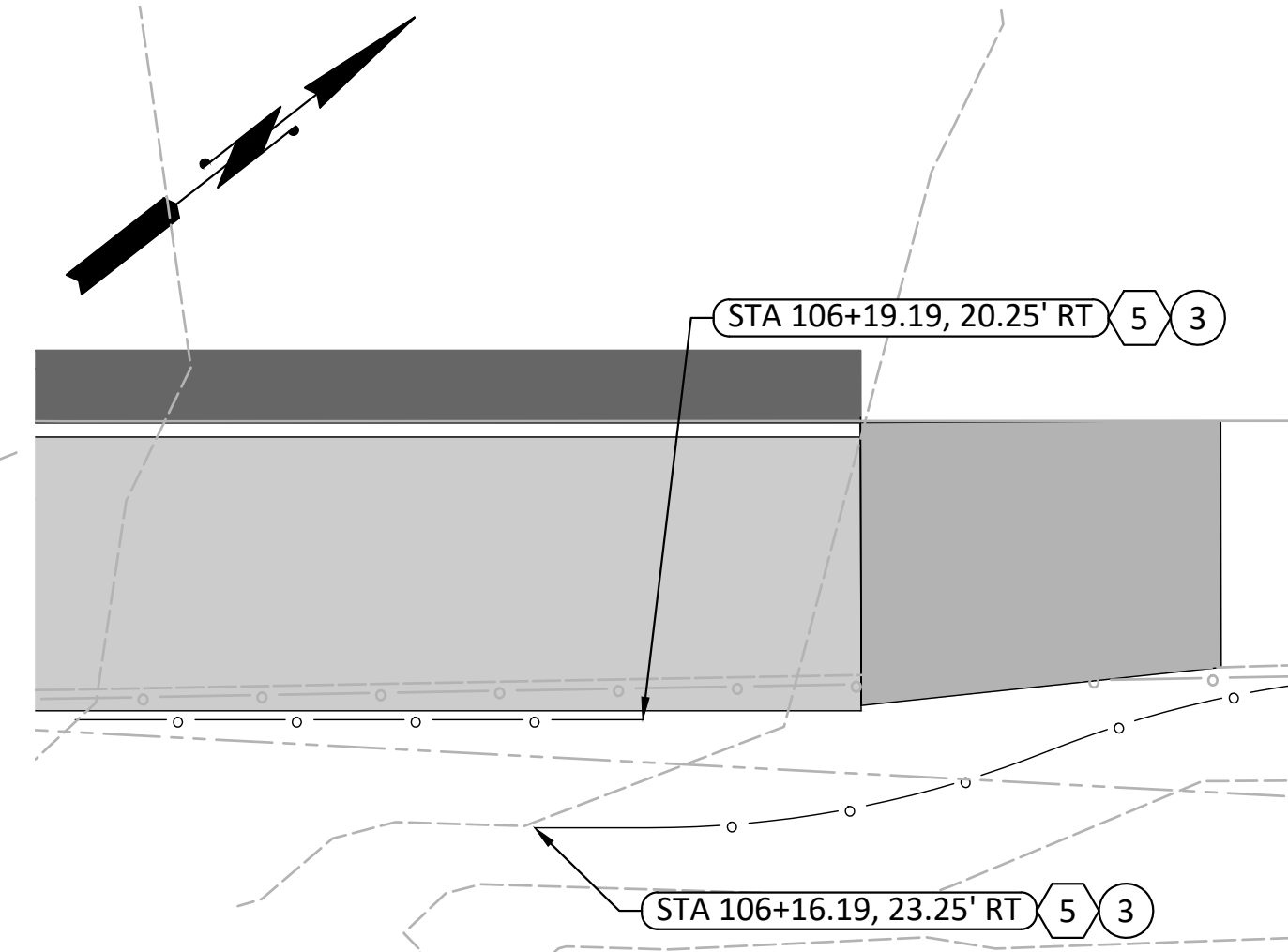
- REFERENCE DRAWING NO. G-4 FOR ALIGNMENT GEOMETRY TABLE.

**KEY NOTES:**

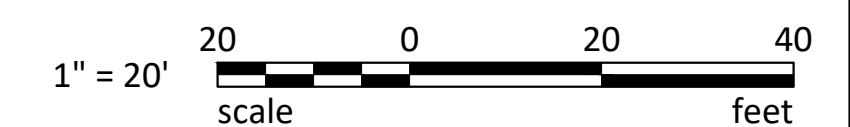
- 1 PAINT LINE
- 2 CEMENT CONC. TRAFFIC CURB; REFERENCE DETAIL 2, DRAWING NO. RS-X
- 3 ASPHALT CONC. RAMP; REFERENCE DETAIL X, DRAWING NO. D-X
- 4 CEMENT CONC. SIDEWALK
- 5 BEAM GUARDRAIL TYPE 31
- 6 PERMANENT SIGNING

**CONSTRUCTION NOTES:**

- 1 4" WIDE WHITE PLASTIC LINE. EXTEND 20.0' FROM STOP LINE; REFERENCE DETAIL X, DRAWING NO. D-X
- 2 PROTECT EXIST. SIGN
- 3 BEGIN/END GUARDRAIL
- 4 INSTALL W11-2 PEDESTRIAN CROSSING ROAD SIGN ABOVE W16-P9 "AHEAD" SIGN ON STEEL SIGN POST
- 5 NOT USED
- 6 YELLOW DOUBLE CENTERLINE. EXTEND 20.0' FROM STOP LINE; REFERENCE DETAIL X, DRAWING NO. D-X
- 7 BEGIN SIDEWALK; MATCH EXIST. CURB ELEVATION
- 8 END CEMENT CONC. SIDEWALK/BEGIN ASPHALT RAMP
- 9 INSTALL W11-2 PEDESTRIAN CROSSING ROAD SIGN ABOVE W16-P9 "AHEAD" SIGN ON EXISTING LIGHT POLE ABOVE EXISTING SIGN



**1 PLAN INSERT**  
SCALE: 1" = 5'



**50% DESIGN**

MARK	REVISION DESCRIPTION	BY	APP.	DATE

WSP USA Inc.  
700 NE Multnomah Street  
Suite 500  
Portland, OR 97232-4120  
TEL: 503-872-4100  
FAX: 503-872-4101

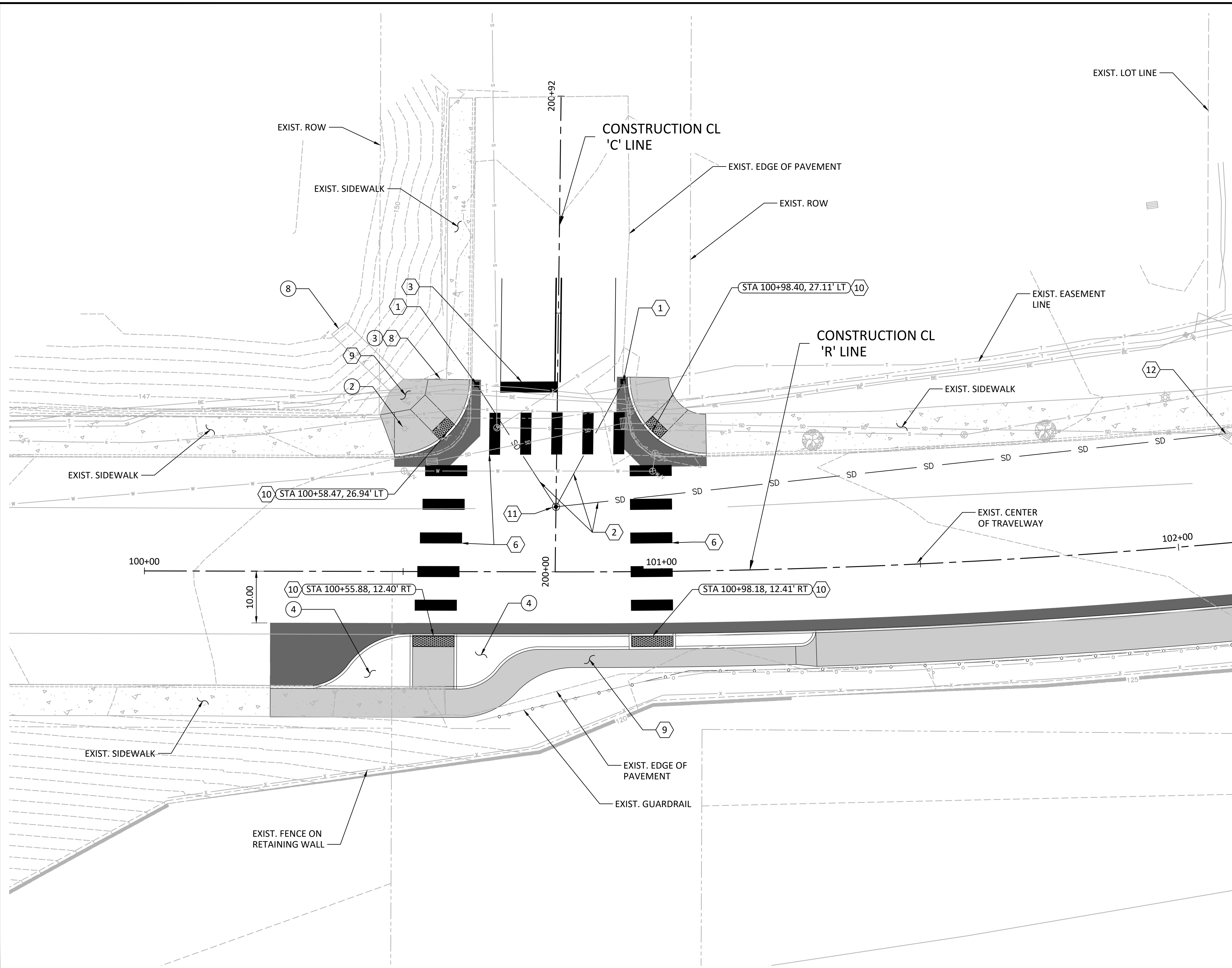
STEVENSON  
City of Stevenson

DRAWN BY	JTH
DESIGN BY	VBV
CHECK BY	JRB
PROJ MGR	VBV

**CITY OF STEVENSON**  
**1ST STREET PEDESTRIAN AMENITIES AND OVERLOOK PROJECT**  
**1ST STREET PLAN**

DRAWING NO.	<b>C-1</b>
PROJECT NO.	30900230_000
DATE:	7/30/20
SHEET NO.	9 OF 21





**GENERAL NOTES:**

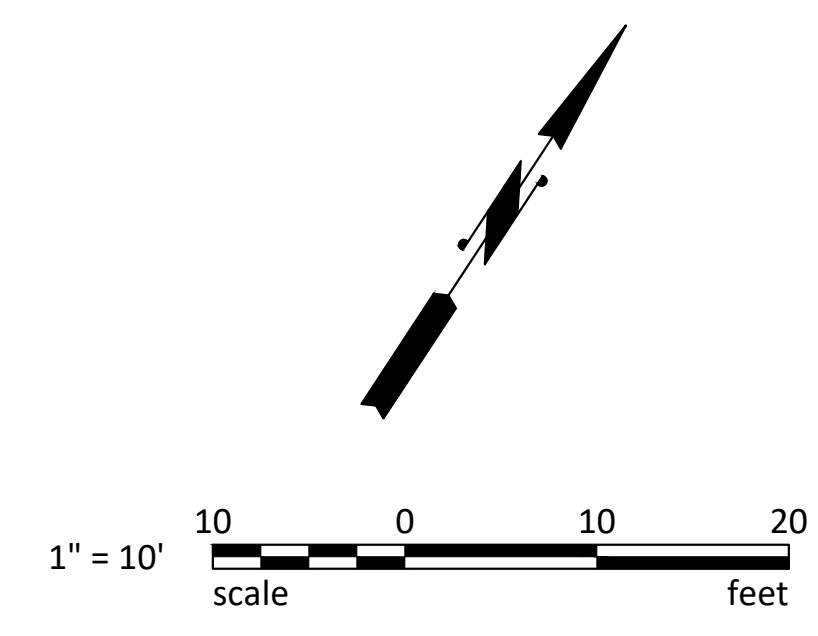
1. REFERENCE DRAWING NO. G-4 FOR ALIGNMENT GEOMETRY TABLE.

**KEY NOTES:**

- 1 CATCH BASIN TYPE 1; REFERENCE DETAIL X, DRAWING NO. D-X
- 2 SOLID WALL PVC STORM SEWER PIPE 8 IN. DIAM.
- 3 PLASTIC STOP LINE; REFERENCE DETAIL X, DRAWING NO. D-X
- 4 HMA CL 1/2 IN. PG 64-22
- 5 PLASTIC STOP LINE; REFERENCE DETAIL X, DRAWING NO. D-XX
- 6 PLASTIC CROSSWALK LINE; REFERENCE DETAIL X, DRAWING NO. D-XX
- 7 PLASTIC LINE
- 8 PERMANENT SIGNING
- 9 CEMENT CONC. SIDEWALK
- 10 CEMENT CONC. CURB RAMP TYPE PERPENDICULAR; REFERENCE DETAIL X, DRAWING NO. D-X
- 11 CATCH BASIN TYPE 2, 48 IN. DIAM.; REFERENCE DETAIL X, DRAWING NO. D-X
- 12 CONNECT TO EXISTING STRUCTURE

**CONSTRUCTION NOTES:**

- 1 MATCH EXISTING SIDEWALK WIDTH
- 2 PROTECT LIGHT POLE IN PLACE
- 3 INSTALL R1-1 "STOP" SIGN
- 4 PLANTED AREA; REFERENCE DRAWING NO. PL-01
- 5 BEGIN/END 4" WIDE WHITE PAINT LINE; REFERENCE DETAIL XX, DRAWING NO. D-XX
- 6 BEGIN/END 4" WIDE DOUBLE CENTERLINE; REFERENCE DETAIL XX, DRAWING NO. D-XX
- 7 PROTECT SIGN IN PLACE
- 8 PROTECT EXISTING STAIRS



**50% DESIGN**

MARK	REVISION DESCRIPTION	BY	APP.	DATE

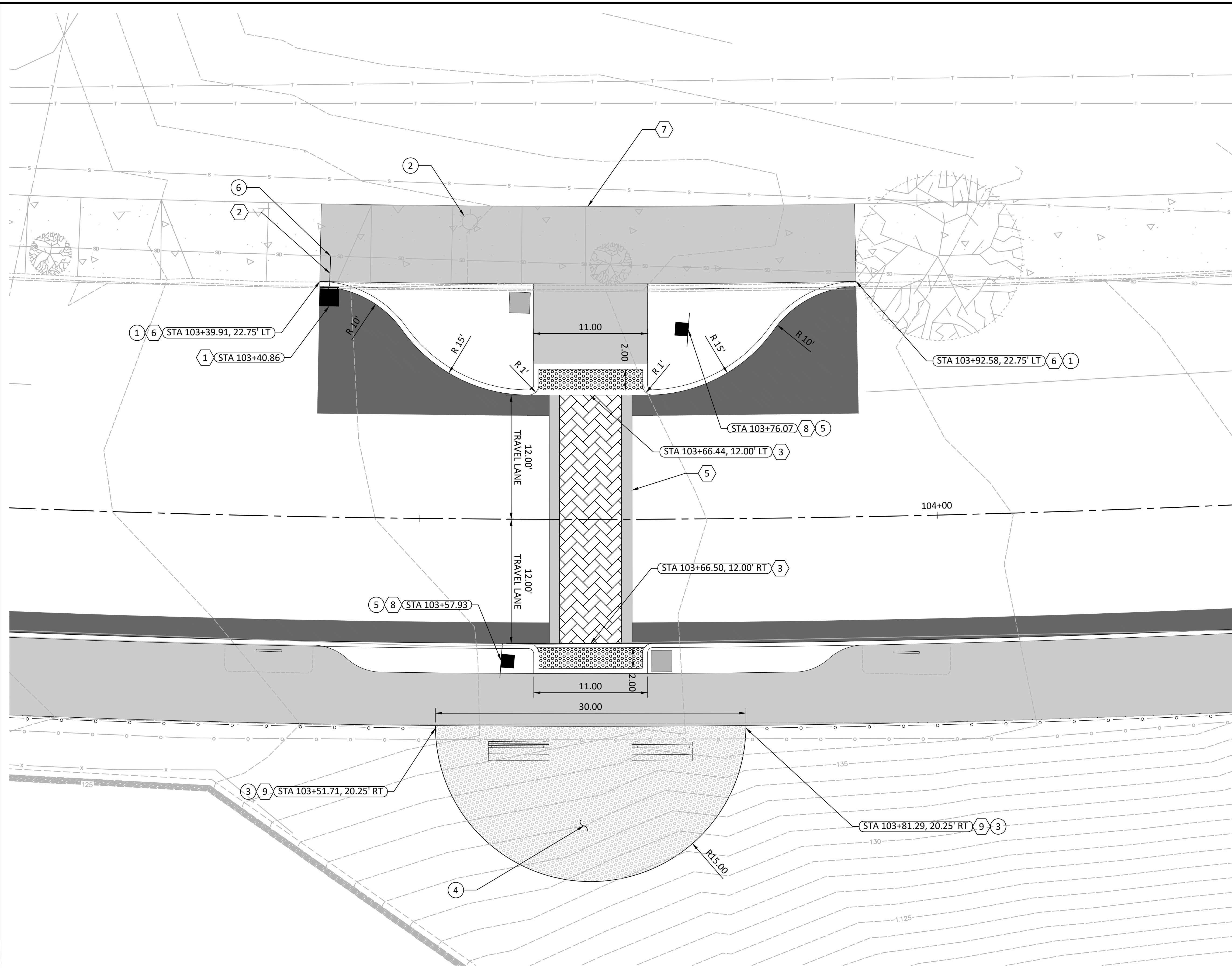
WSP USA Inc.  
700 NE Multnomah Street  
Suite 500  
Portland, OR 97232-4120  
TEL: 503-872-4100  
FAX: 503-872-4101

STEVENSON

DRAWN BY	JTH
DESIGN BY	VBV
CHECK BY	JRB
PROJ MGR	VBV

**CITY OF STEVENSON**  
**1ST STREET PEDESTRIAN AMENITIES AND OVERLOOK PROJECT**  
**COLUMBIA STREET INTERSECTION PLAN**

DRAWING NO.	<b>C-2</b>
PROJECT NO.	30900230_000
DATE:	7/30/20
SHEET NO.	10 OF 21



**GENERAL NOTES:**

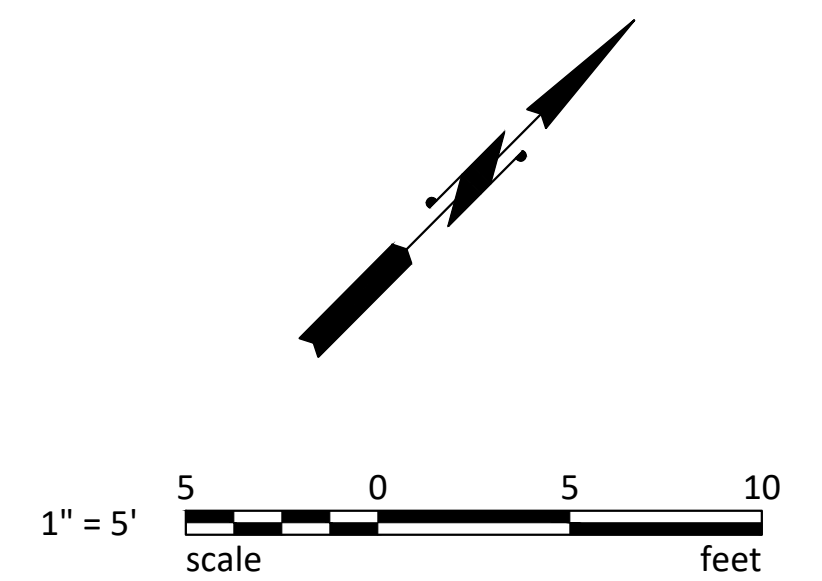
1. REFERENCE DRAWING NO. G-4 FOR ALIGNMENT GEOMETRY TABLE.

**KEY NOTES:**

- 1 CATCH BASIN TYPE 1; REFERENCE DETAIL X, DRAWING NO. D-X
- 2 SOLID WALL PVC STORM SEWER PIPE 8 IN. DIAM.
- 3 CEMENT CONC. CURB RAMP TYPE PERPENDICULAR; REFERENCE DETAIL 2, DRAWING NO. D-X
- 4 HMA CL 1/2 IN. PG 64-22
- 5 CEMENT CONC. STAMPED CROSSWALK
- 6 CEMENT CONC. TRAFFIC CURB; REFERENCE DETAIL 2, DRAWING NO. RS-2
- 7 CEMENT CONC. SIDEWALK
- 8 PERMANENT SIGNING
- 9 BEAM GUARDRAIL TYPE 31

**CONSTRUCTION NOTES:**

- 1 MATCH EXISTING SIDEWALK WIDTH AND CURB ELEVATION
- 2 PROTECT LIGHT POLE IN PLACE
- 3 BEGIN/END GUARDRAIL
- 4 STRUCTURAL OVERLOOK, REFERENCE DRAWING NO. S-2
- 5 INSTALL W11-2 PEDESTRIAN CROSSING ROAD SIGN ABOVE W16-7P DIAGONAL BLACK ARROW POINTING DOWN SIGN ON NEW STEEL POST
- 6 CONNECT TO EXISTING PVC STORM SEWER PIPE



**50% DESIGN**

MARK	REVISION DESCRIPTION	BY	APP.	DATE

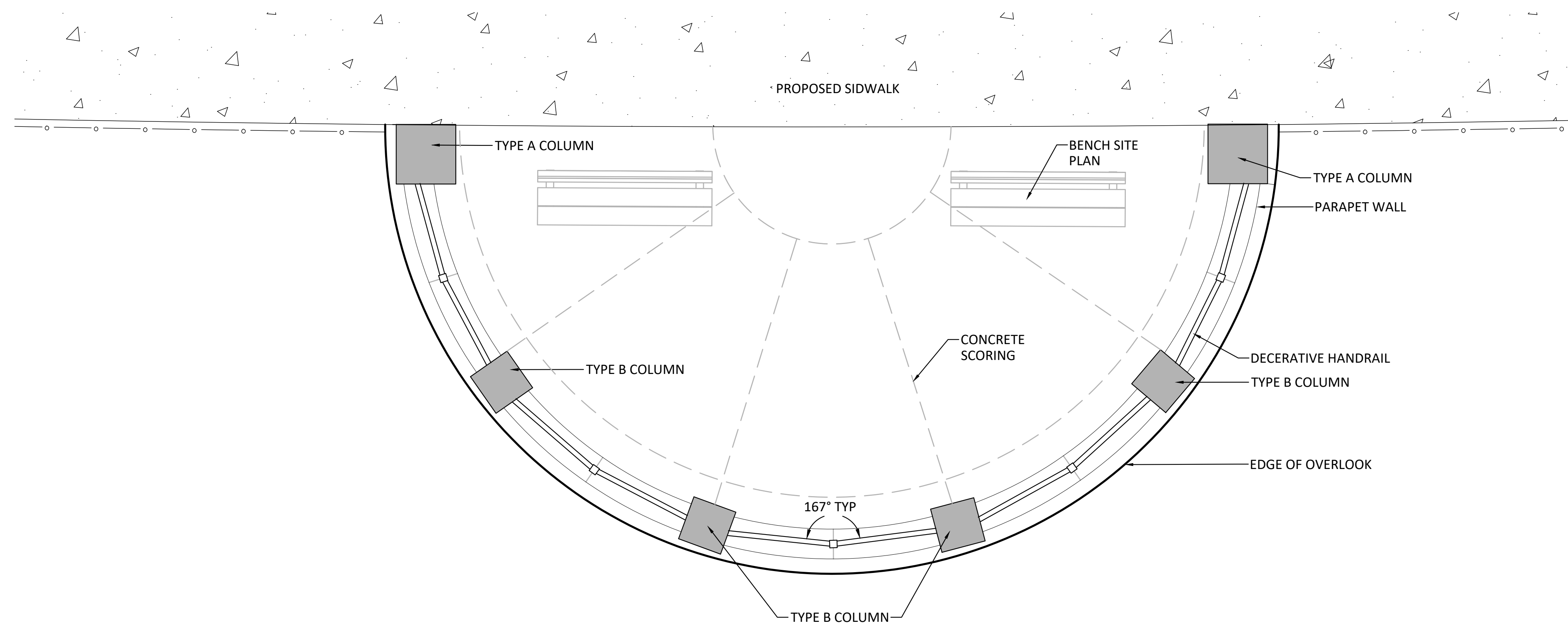
WSP USA Inc.  
700 NE Multnomah Street  
Suite 500  
Portland, OR 97232-4120  
TEL: 503-872-4100  
FAX: 503-872-4101

STEVENSON

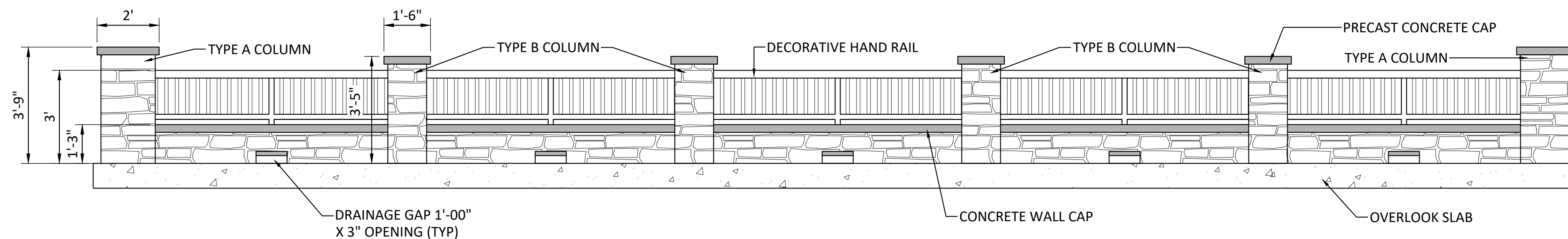
DRAWN BY JTH  
DESIGN BY VBV  
CHECK BY JRB  
PROJ MGR VBV

**CITY OF STEVENSON**  
**1ST STREET PEDESTRIAN AMENITIES AND OVERLOOK PROJECT**  
**OVERLOOK CROSSING PLAN**

DRAWING NO. **C-3**  
PROJECT NO. 30900230\_000  
DATE: 7/30/20  
SHEET NO. 11 OF 21



**OVERLOOK PLAN VIEW**  
SCALE: NTS



**OVERLOOK HANDRAIL ELEVATION**  
SCALE: NTS

**50% DESIGN**

MARK	REVISION DESCRIPTION	BY	APP.	DATE

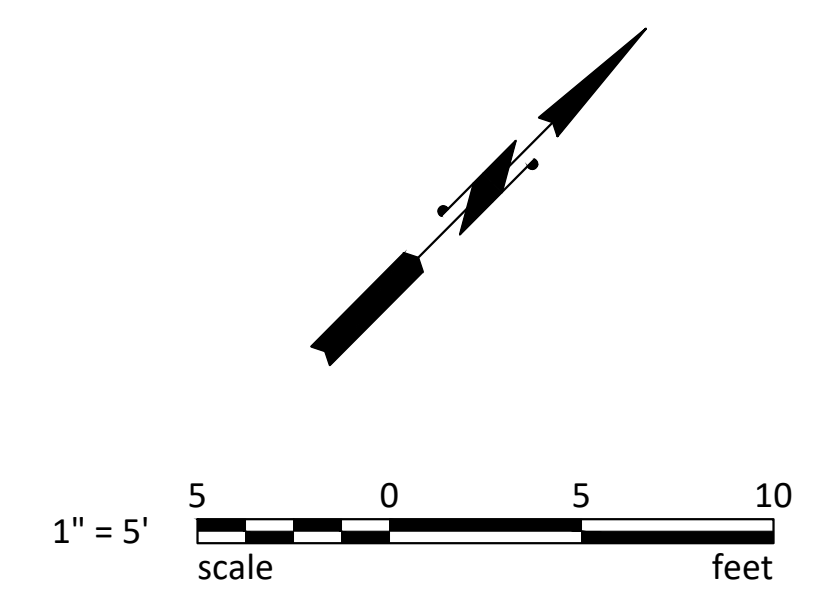
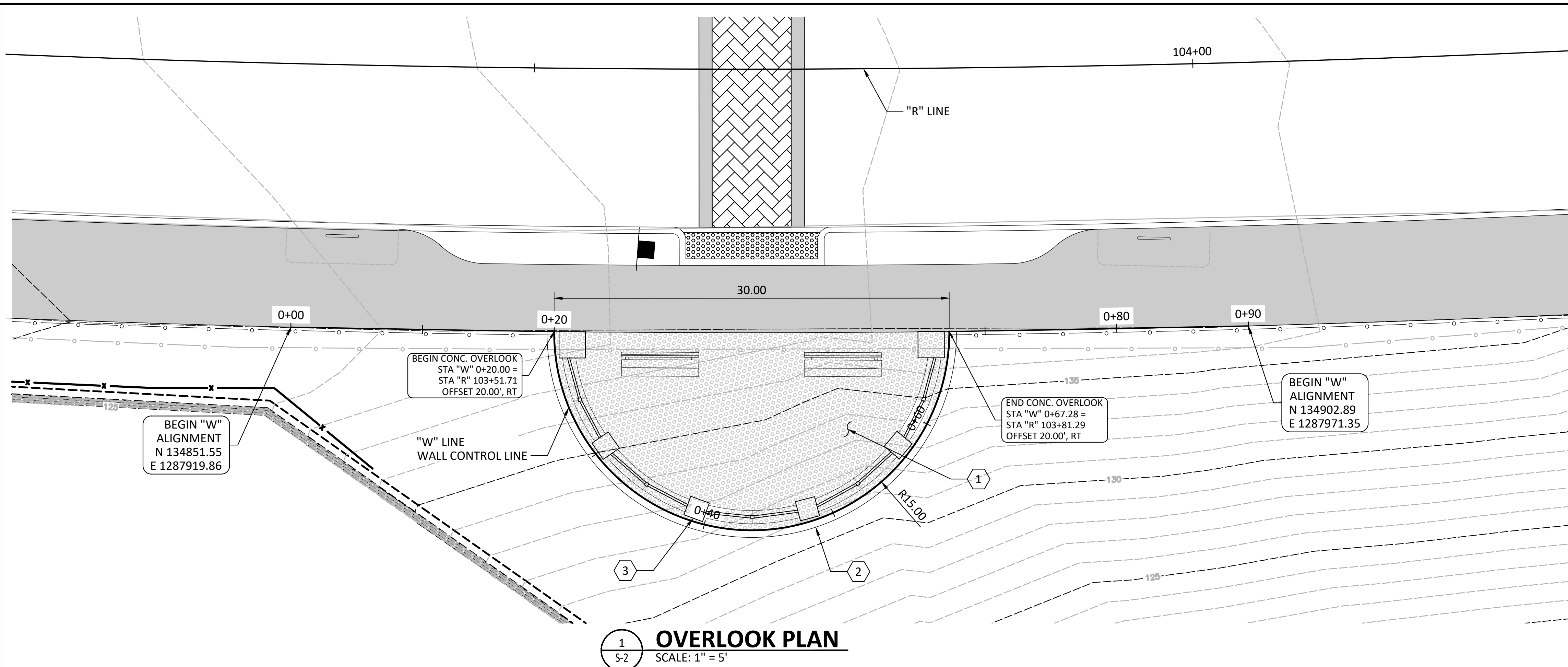
WSP USA Inc.  
700 NE Multnomah Street  
Suite 500  
Portland, OR 97232-4120  
TEL: 503-872-4100  
FAX: 503-872-4101



DRAWN BY JTH  
DESIGN BY VBV  
CHECK BY JRB  
PROJ MGR VBV

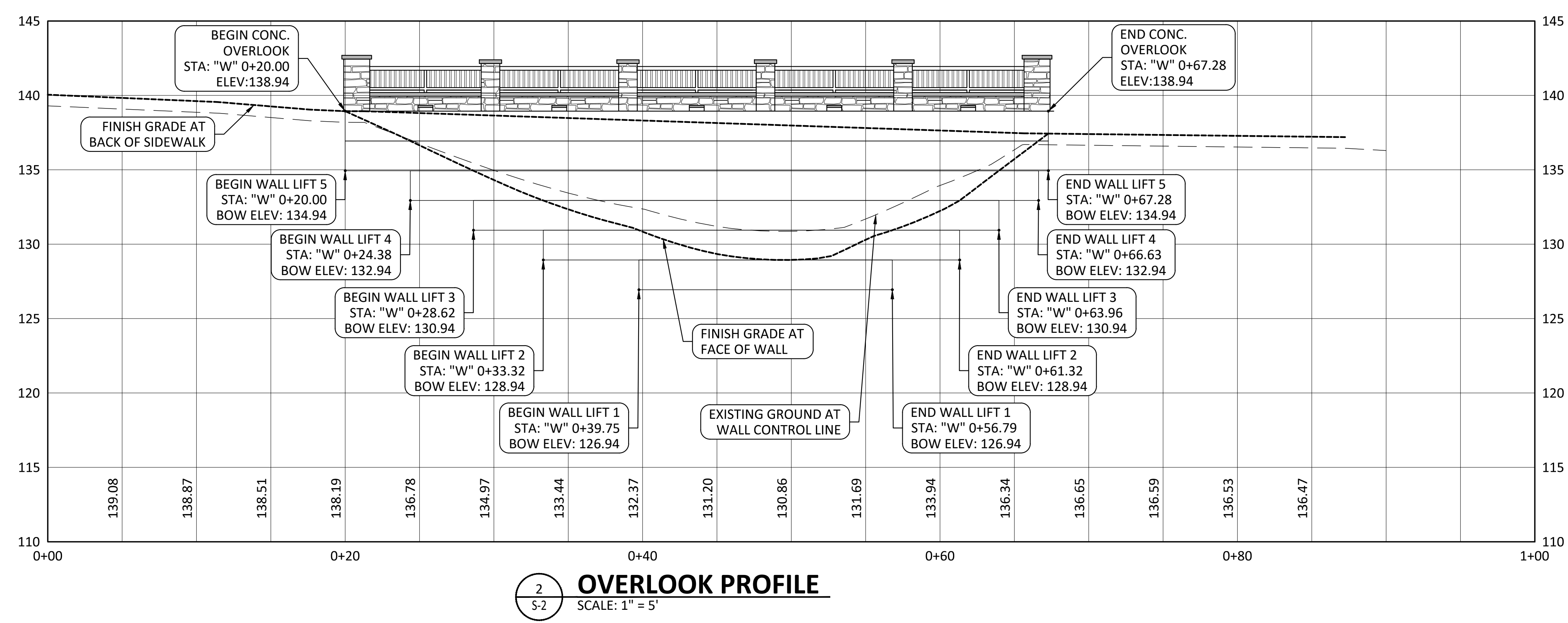
**CITY OF STEVENSON**  
**1ST STREET PEDESTRIAN AMENITIES AND OVERLOOK PROJECT**  
OVERLOOK HANDRAIL PLAN AND ELEVATION

DRAWING NO. **D-1**  
PROJECT NO. 30900230\_000  
DATE: 7/30/20  
SHEET NO. 12 OF 21



**KEY NOTES:**

- ① CEMENT CONC. OVERLOOK SLAB-ON-GRADE
- ② MSE RETAINING WALL WITH WIRE BASKET AND ROCK FACING
- ③ STONE RAILING, SEE SHEET D01



**50% DESIGN**

MARK	REVISION DESCRIPTION	BY	APP.	DATE

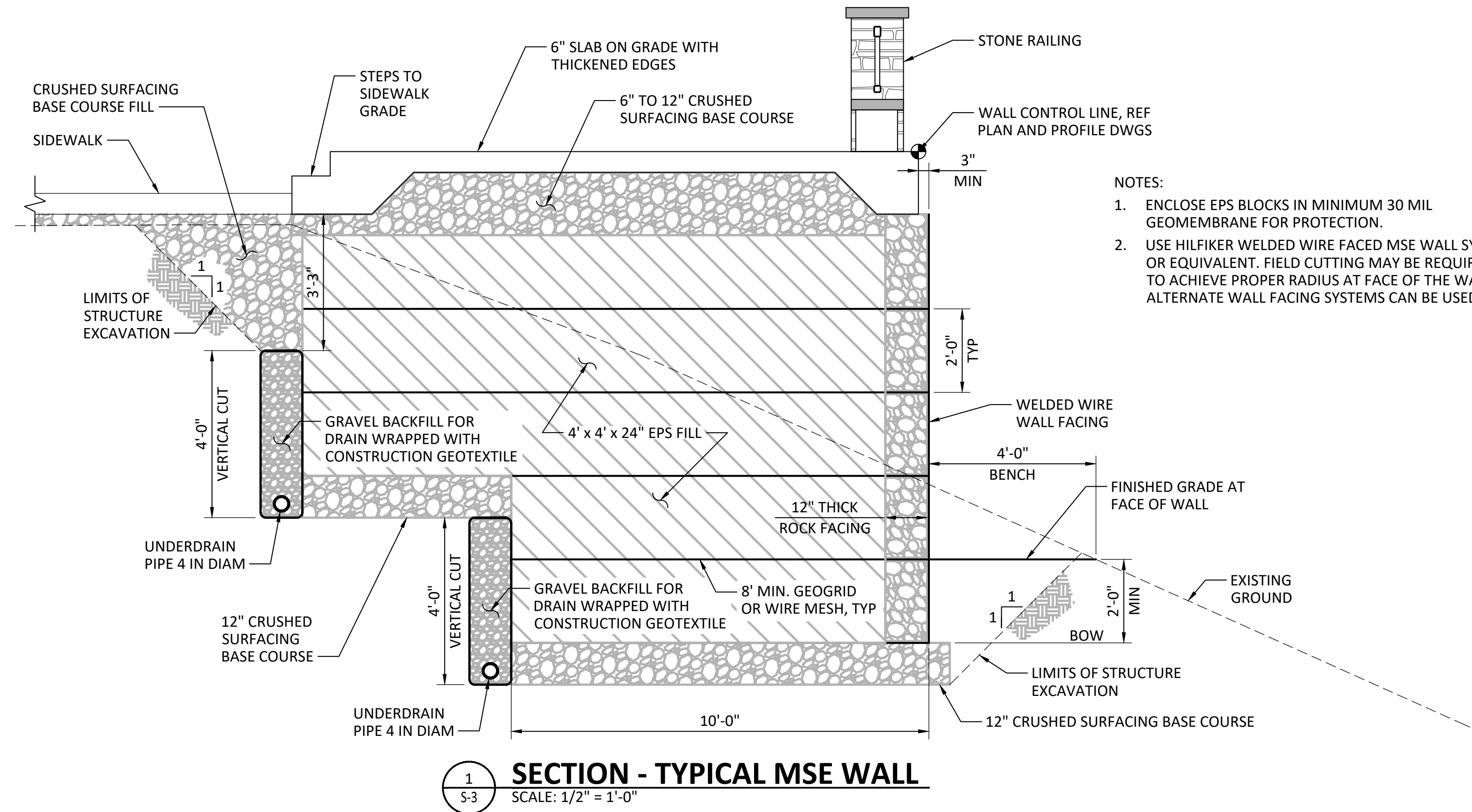
WSP USA Inc.  
700 NE Multnomah Street  
Suite 500  
Portland, OR 97232-4120  
TEL: 503-872-4100  
FAX: 503-872-4101



DRAWN BY JTH  
DESIGN BY VBV  
CHECK BY JRB  
PROJ MGR VBV

**CITY OF STEVENSON**  
**1ST STREET PEDESTRIAN AMENITIES AND OVERLOOK PROJECT**  
  
OVERLOOK PLAN AND PROFILE

DRAWING NO. **S-2**  
PROJECT NO. 30900230\_000  
DATE: 7/30/20  
SHEET NO. 14 OF 21



- NOTES:
1. ENCLOSE EPS BLOCKS IN MINIMUM 30 MIL GEOMEMBRANE FOR PROTECTION.
  2. USE HILFER WELDED WIRE FACED MSE WALL SYSTEM OR EQUIVALENT. FIELD CUTTING MAY BE REQUIRED TO ACHIEVE PROPER RADIUS AT FACE OF THE WALL. ALTERNATE WALL FACING SYSTEMS CAN BE USED.

MARK	REVISION DESCRIPTION	BY	APP.	DATE

WSP USA Inc.  
700 NE Multnomah Street  
Suite 500  
Portland, OR 97232-4120  
TEL: 503-872-4100  
FAX: 503-872-4101

STEVENSON  
City of Stevenson

DRAWN BY JTH  
DESIGN BY VBV  
CHECK BY JRB  
PROJ MGR VBV

CITY OF STEVENSON  
**1ST STREET PEDESTRIAN AMENITIES AND OVERLOOK PROJECT**  
OVERLOOK DETAILS

**50% DESIGN**

DRAWING NO. **S-3**  
PROJECT NO. 30900230\_000  
DATE: 7/30/20  
SHEET NO. 15 OF 21



**Washington State  
Department of Transportation**

<b>CITY/TOWN TURNBACK AGREEMENT</b>			ORGANIZATION AND ADDRESS  City of Stevenson P.O. Box 371 Stevenson, WA 98648
AGREEMENT NUMBER <b>TB 4-0063</b>			SECTION/LOCATION SR 14, City of Stevenson Stevenson Two Way Couplet
STATE ROUTE NUMBER 14	CONTROL SECTION NUMBER 3002 3003	DISTRICT NUMBER SW	

THIS AGREEMENT, made and entered into this 25<sup>th</sup> day of JUNE, 19 96, between the STATE OF WASHINGTON, Department of Transportation, acting by and through the Secretary of Transportation, hereinafter called the "STATE", and the above named organization, hereinafter called the "CITY".

WHEREAS, the STATE is planning the construction or improvement of a section of state route as shown above, and

WHEREAS, the STATE will abandon a portion of an existing state highway and/or construct, reconstruct, or rearrange certain city streets, frontage roads, access roads, intersections, ramps, crossings and/or other pertinent features, and

WHEREAS, construction of this facility including rearranged city streets and other features will necessitate the transfer of jurisdiction from the CITY to the STATE of those rights of way, and

WHEREAS, upon completion of construction of this facility it is necessary to describe the division of responsibility of the STATE and CITY in the ownership, maintenance and reconstruction of this roadway and other features, and provide for the transfer of rights accordingly.

NOW THEREFORE, by virtue of Title 47.24.010 and 47.52.210, and pursuant to WAC 468-18-050, "Policy on the construction, improvement and maintaining of intersections of state highways and city streets", WAC 468-30-075, "Procedure for transfer of abandoned state highways to cities and towns" and in consideration of the terms, conditions, covenants and performance contained herein or attached and incorporated and made a part hereof, IT IS MUTUALLY AGREED AS FOLLOWS:

**I  
CITY RIGHT OF WAY**

The CITY agrees that the transfer of jurisdiction and/or ownership of all city rights of way needed for the construction of this facility shall be governed by R.C.W. 47.24.010 and/or R.C.W. 47.52.210. Prior to commencement of construction, the STATE will notify the CITY in writing that it will assume jurisdiction and relieve the CITY from all responsibility in the operation, maintenance and reconstruction thereon until construction is complete.

**II  
ABANDONED STATE HIGHWAY**

If a public highway which is or has been a part of the route of a state highway and is no longer necessary as such is to be certified to the CITY, it will be included in the plans marked Exhibit "A", attached hereto and by this reference made a part of this AGREEMENT.

In those cases involving abandonment of a state highway, a joint maintenance inspection by representatives of the STATE and CITY shall be held prior to entering into this AGREEMENT and all agreed to deficiencies, if any, shall be enumerated in detail and included as part of Exhibit "A".

Items of work, if any, which cannot be agreed to prior to executing this AGREEMENT, shall also be enumerated in detail and included as part of Exhibit "A".

**III  
COMPLETION INSPECTION**

Upon completion of construction of the facilities covered by this AGREEMENT, an inspection by representatives of the STATE and CITY shall be made to determine that the requirements of this AGREEMENT have been fulfilled.

The CITY, following satisfactory completion of the joint inspection, will provide the STATE a letter agreeing to accept the facilities covered by this AGREEMENT in their present condition.

**IV  
TRANSFER OF JURISDICTION**

Subsequent to the completion of construction, opening to public use and receipt of the CITY's letter of acceptance, the STATE will notify the CITY in writing of its intent to transfer jurisdiction of these features as shown on the attached plans marked Exhibit "A". The CITY agrees to accept said abandoned highway, rearranged city streets, frontage roads, cul-de-sacs, and other features, including right of way, access control and other property rights, and to relieve the STATE from all responsibilities in the operation, maintenance, and reconstruction of these features. Exhibit "A" is colored, wherever applicable, as follows:

**Red** indicates construction and rights of way to be conveyed to the CITY.

- Blue** indicates easements to be conveyed to the CITY.
- Yellow** indicates non-operating properties to be conveyed to the CITY. These properties are considered necessary for the continued maintenance of the areas shown in red and/or blue color.
- Green** indicates areas within the highway right of way to be maintained and reconstructed by the CITY. Except for snow and ice removal, maintenance and reconstruction of the separation structures shall be the responsibility of the STATE.
- Orange** indicates access control and access rights to be conveyed to the CITY. These rights may be maintained or disposed of by the CITY and any revenue resulting from said disposal shall be placed in the CITY's street fund and used exclusively for street purposes.
- Brown** indicates access control and access rights to be conveyed to the CITY. These rights shall be maintained by the CITY and will not be transferred, sold, abandoned, vacated, or otherwise altered or disposed of without prior written approval of the STATE.

**V  
RECORDED CONVEYANCE**

Within six months following the notice to transfer jurisdiction, the STATE will furnish the CITY a recorded conveyance of those features shown in red, blue, yellow, orange and/or brown color on the plans marked Exhibit "A".

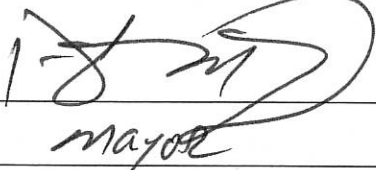
The forthcoming instrument will be subject to the following restrictions:

- \* ~~It is understood and agreed that the above described property is transferred for street purposes, and that all revenue resulting from the vacation of said street, and the sale or rental of such property, shall be placed in the CITY's street fund and used exclusively for street purposes.~~

**VI  
LEGAL RELATIONS**

No liability shall attach to the STATE or CITY by reason of entering into this AGREEMENT except as expressly provided herein.

IN WITNESS WHEREOF the parties hereto have executed this AGREEMENT on the day and year first above written.

CITY  
By:   
Mayor

STATE OF WASHINGTON  
DEPARTMENT OF TRANSPORTATION  
By:   
District Administrator

Date: 06-25-96

\* It is understood and agreed that the above referenced property is transferred for road purposes only, and no other use shall be made of said property without the prior written approval of the Grantor. It is also understood and agreed that the Grantee, its successors or assigns, shall not revise either the right of way lines or the access control without prior written approval from the Grantor, its successors or assigns. Revenues resulting from any vacation, sale, or rental of this property or any portion thereof, shall (1) if the property is disposed of to a governmental entity for public use, be placed in the Grantee's road fund and used exclusively for road purposes; or (2) if the property is disposed of other than as provided in (1) above, be shared by the Grantee and Grantor, their successors, or assigns in the same proportion as acquisition costs were shared, except that the Grantee may deduct the supportable direct costs of any such vacation, sale, or rental.



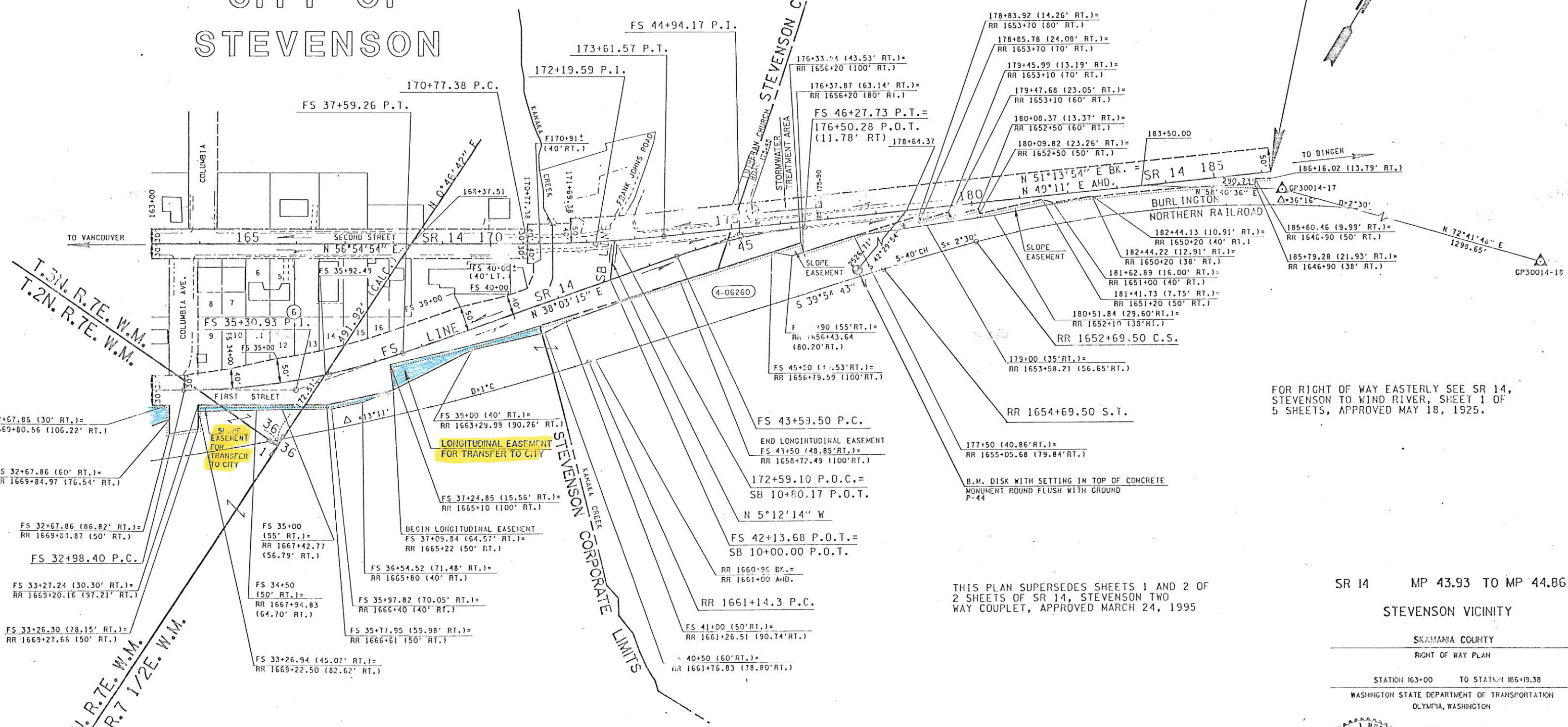


REGION NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.
0	WASH.	DPR-0078(001)	

CURVE DATA				
I. STATION	DELTA	RADIUS	TANGENT	LENGTH
72+19.59	5°41'00" LT.	2865'	142.21'	284.19'
35+30.93	18°51'39" LT.	1400'	232.53'	460.86'
44+94.17	12°48'25" RT.	1200'	134.67'	268.23'

# CITY OF STEVENSON

END OF PLAN  
 STA. 186+19.38 P.O.T. BK. =  
 STA. 72+00 P.O.T. AHD.  
 MP 44.86



FOR RIGHT OF WAY EASTERLY SEE SR 14, STEVENSON TO WIND RIVER, SHEET 1 OF 5 SHEETS, APPROVED MAY 18, 1925.

THIS PLAN SUPERSEDES SHEETS 1 AND 2 OF 2 SHEETS OF SR 14, STEVENSON TWO WAY COUPLET, APPROVED MARCH 24, 1995

SR 14 MP 43.93 TO MP 44.86  
 STEVENSON VICINITY

SKAMANIA COUNTY  
 RIGHT OF WAY PLAN

STATION 163+00 TO STATION 186+19.38  
 WASHINGTON STATE DEPARTMENT OF TRANSPORTATION  
 OLYMPIA, WASHINGTON



All Plans are subject to change. For the most current information please refer to the file in the Engineering Department in Olympia.

APPROVED FEBRUARY 23, 1996  
 SHEET 2 OF 2 SHEETS

RIVERVIEW ADDITION TO THE TOWN OF STEVENSON

SEE SHEET NO. 1	TOTAL AREA	F/V	LT. REMAINING RT.	EASHT

**LEGEND**

PROPERTY OWNERSHIP NUMBERS (4-04000)

PROPERTY LINES

Scale: 0 100 200 FEET

## EXHIBIT A TB 4-0063

Letter 1-19-96	5-1-96	Noted Slope Easement for Transfer to City on RL Vic. Sta. FS 34+00; Noted Longitudinal Easement for Transfer to City on PL Vic. Sta. FS 33+00	2/0
Letter 3-28-96	4-23-96	Added Stormwater Treatment Area on LL Sta. 175+45 to 176+90	HEP
Letter 11-1-96	1-1-96	Subsequent Approval	By

# Reconnect to the River

## Capitalize on Downtown's Unique Setting.

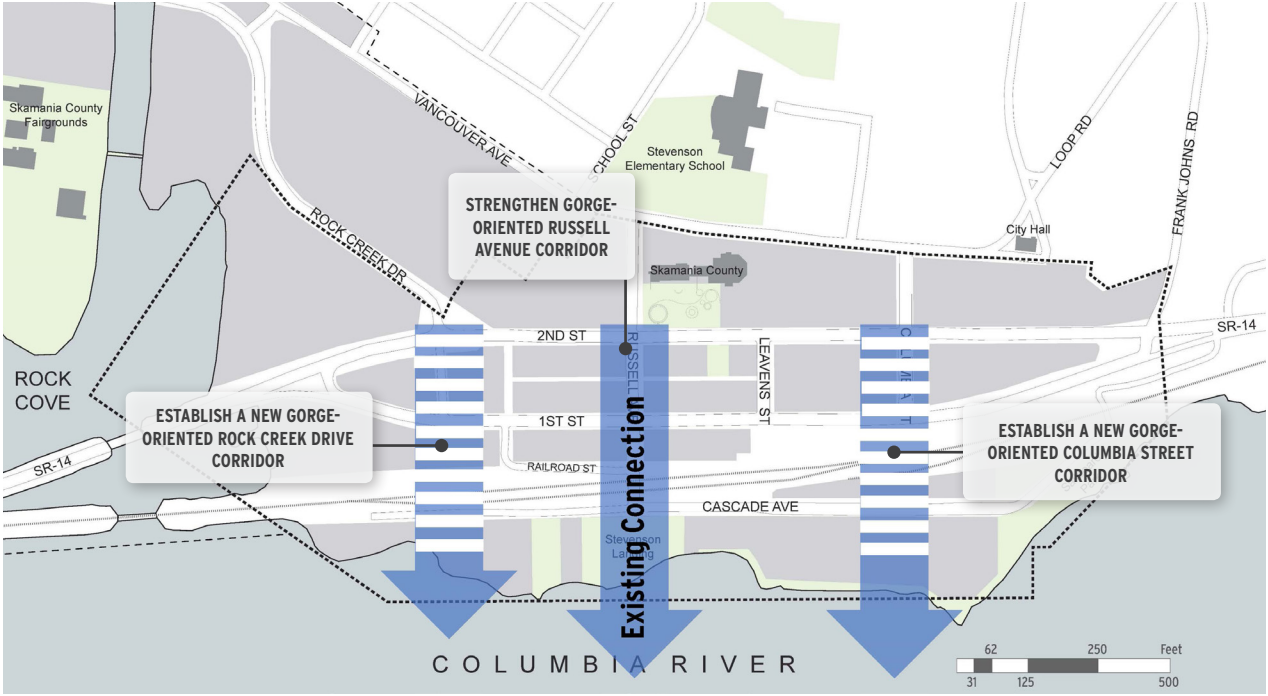
Success means strengthening and building upon the natural environment—the Columbia River Gorge. Stevenson's very existence is intertwined with the Columbia River. Providing better access, both visually and physically, is important historically, spiritually, and culturally, as well as economically.

Over time, downtown's linkage to the river has been weakened by barriers and planning decisions that have in many instances resulted in development turning a shoulder to the Gorge. A concerted effort to reimagine and repair the this connection with a series of corridors that feature the Gorge will attract visitors and residents alike.

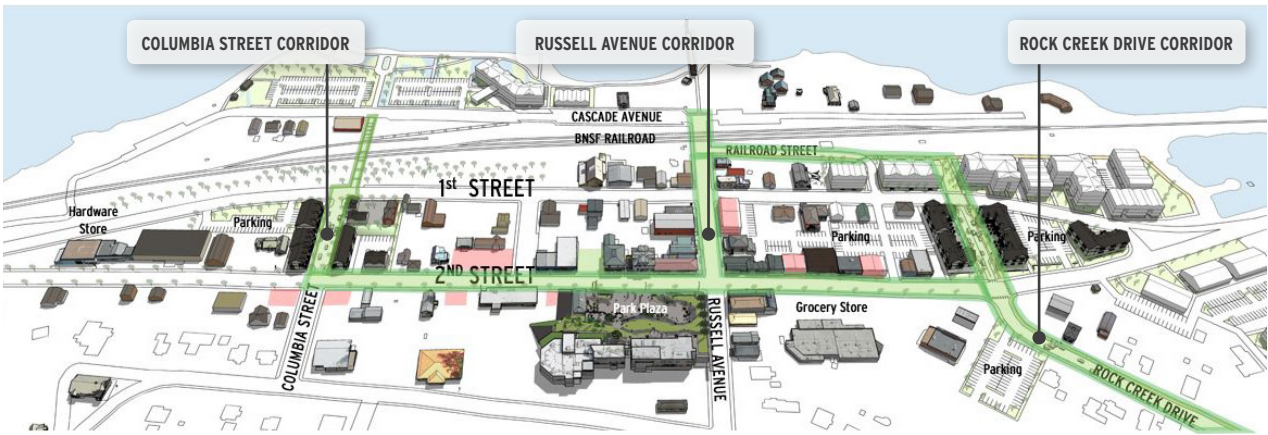
The *Reconnect to the River* elements are:

- » **Strengthen Gorge-Oriented Russell Avenue Corridor.** Implement all planned street enhancements.
- » **Establish a New Gorge-Oriented Rock Creek Drive Corridor.** Provide new view corridor, roadway, walking, and biking linkage via Railroad Street.
- » **Establish a New Gorge-Oriented Columbia Street Corridor.** Enhance view corridor, enhanced roadway, and new walking and biking linkage over railroad tracks.

## RECONNECT TO THE RIVER



## ESTABLISH GORGE-ORIENTED CORRIDORS (LOOKING SOUTH)



## RUSSELL AVENUE



### **Strengthen Gorge-Oriented Russell Avenue Corridor**

*Implement all planned street enhancements.*

Success means strengthening the existing Gorge 'window' connection between the commercial core, the river, and Stevenson Landing. The physical improvements of the Russell Rebuild Project are important to complete. Other improvements should be advanced as well, and may include 'pedestrian accelerator' projects that make it easier and more desirable for those leaving cruise lines to easily access current and future businesses.

## ROCK CREEK DRIVE EXTENSION



### **Establish a New Gorge-Oriented Rock Creek Drive Corridor**

*Provide new view corridor, roadway, walking, and biking linkage via Railroad Street.*

Success means providing motorists driving along 2nd Street with a view 'window' to the Gorge and development sites. Capturing this drive-by traffic is essential for the economic success of proposed westside mixed-use development along a new extension of Rock Creek Drive. The view window also improves access to new residential development and a direct linkage for those visiting Columbia Gorge Interpretive Center, staying at the Skamania Lodge, or attending an event at the County Fairgrounds along the existing Rock Creek Drive.

## RE-ALIGNED COLUMBIA STREET



### **Establish a New Gorge-Oriented Columbia Street Corridor.**

*Enhance view corridor, enhanced roadway, and new walking and biking linkage over railroad tracks.*

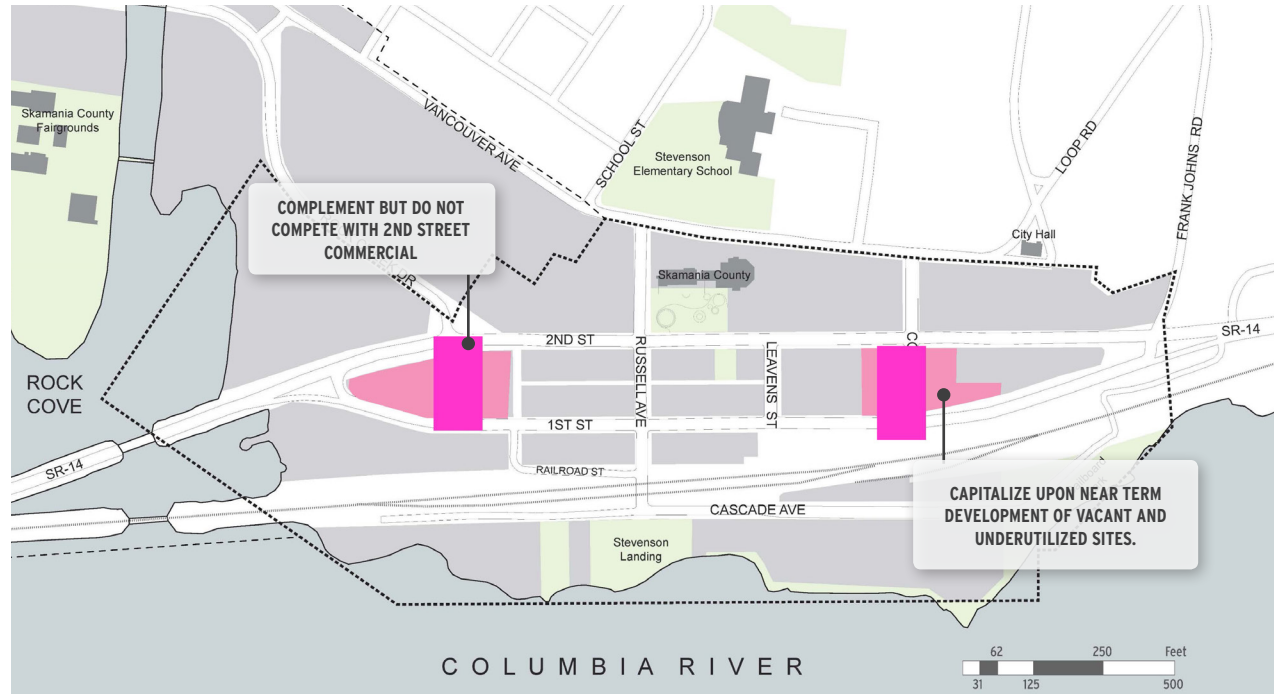
Success means providing motorists driving along 2nd Street with an enhanced view 'window' to the Gorge. Capturing this drive-by traffic is essential for the economic success of proposed mixed-use development along Columbia Street. The window also improves access between 2nd Street and eastside neighborhoods as well as the Port of Skamania. A complete connection will likely need to be phased as it will require more time for coordination and negotiation of an access route over the Burlington Northern Railroad. However, the benefits will be substantial, as it will offer an unobstructed, train-free connection with unmatched views in all directions from high above the trackway.

# Foster Catalyst Projects

*Foster near-term, market driven, achievable development.*

Success means pursuing and developing catalyst projects within a 5-year horizon. While current national and local market trends and demographics are favorable, this condition may not last forever. The current development cycle will slow at some point. Moreover, there are substantial community needs. Currently, a significant affordable multi-family housing 'gap' exists. Urgent action to address the housing gap and achieve a better jobs/housing balance will help resolve social needs as well as result in substantial economic benefits for both residents and businesses.

## FOSTER CATALYST PROJECTS



## FIVE YEAR PROJECTS (LOOKING SOUTH)



## COURTYARD



**Complement but do not compete with 2nd Street commercial.**

*Create a new and unique shopping experience.*

Success means attracting new customers, especially visitors who are currently doing business and spending dollars elsewhere. Since current demand is not extensive, the intent of the Catalyst Projects to make the most of this limited supply by directing it to locations where success won't just be limited to the development site but will spill business over to existing adjacent uses. The two proposed Catalyst Projects along Columbia Street and a new Rock Creek Drive extension will provide the necessary proximity to result in shared success.

## UNDER-UTILIZED SITES



**Capitalize upon near term development of vacant and underutilized sites.**

*Complement existing uses.*

Success means targeting development toward sites where feasible projects can be built with the fewest constraints. Meeting the 5-year timeframe requires focusing on areas with fewer impacts on existing businesses or residents, where adequate land is available to build structures and parking, and where there is willing property owner interest.

# CATALYST PROJECTS

*Foster near-term, market driven, achievable development.*

# Columbia Street



Existing



Concept

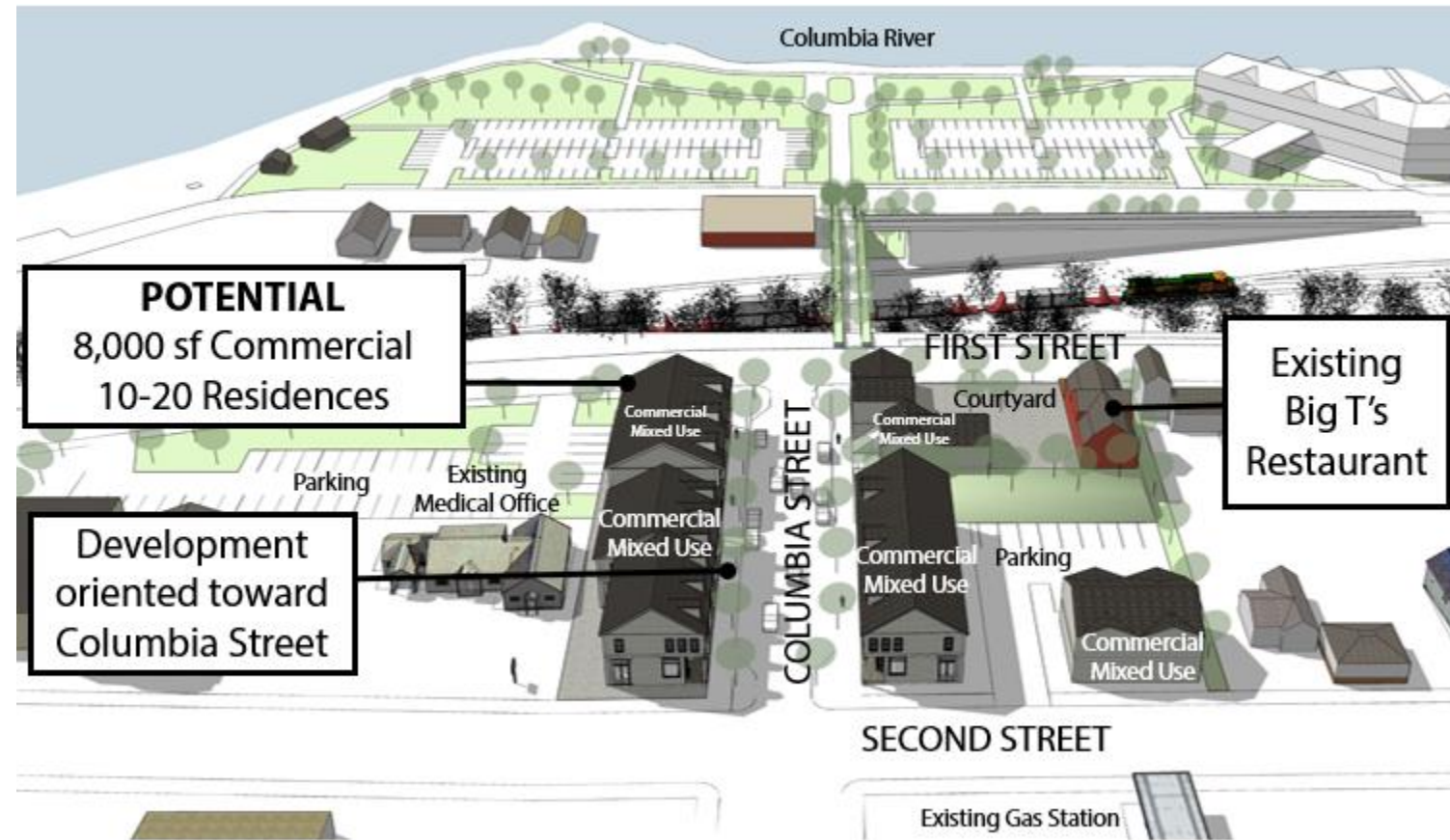
## CATALYST PROJECT

### Economic Assessment:

*Mixed-use development is estimated to be viable, provided it includes:*

- **Commercial:** A single-phase project to 8,000 square feet
- **Residential:** Units on the upper floors. The economics improve as the residential share increases.

# Columbia Street Catalyst Project



Concept

## CATALYST PROJECTS

### Priority and Timing:

*The Columbia Street mixed-use concept to represent a strong catalytic impact and it will enhance Downtown's ability to:*

- **Generate visits**
- **Capture pass-through traffic.**

# Columbia Street Project



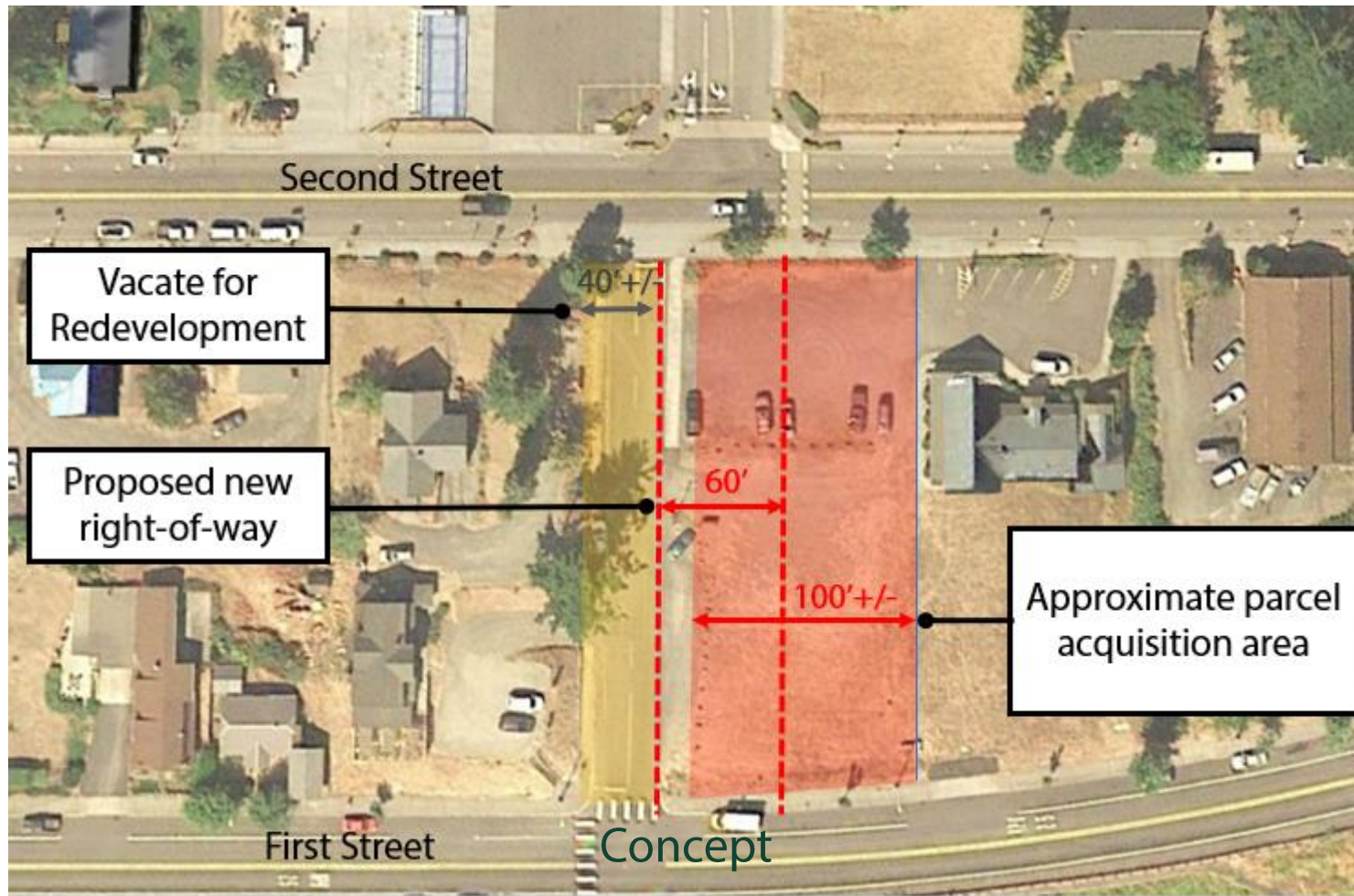
# CATALYST PROJECT

## Additional Actions:

- Traffic Analysis
- Utility Assessment
- Roadway Design and Cost Estimate



# Columbia Street Catalyst Project

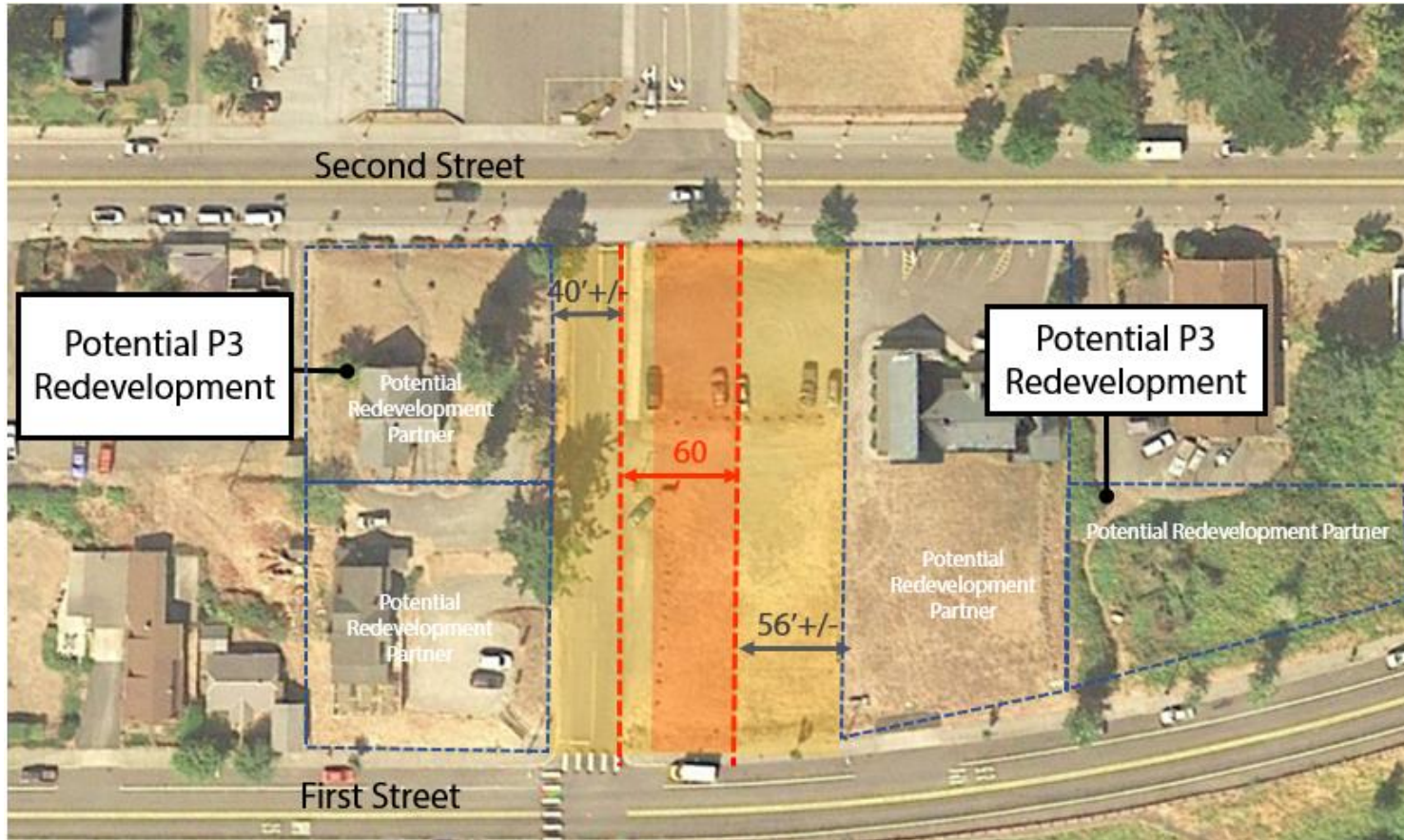


## CATALYST PROJECT

### Additional Actions:

- Traffic Analysis
- Utility Assessment
- Roadway Design and Cost Estimate
- **Right-of-way Acquisition**
- **Funding Options**

# Columbia Street Catalyst Project



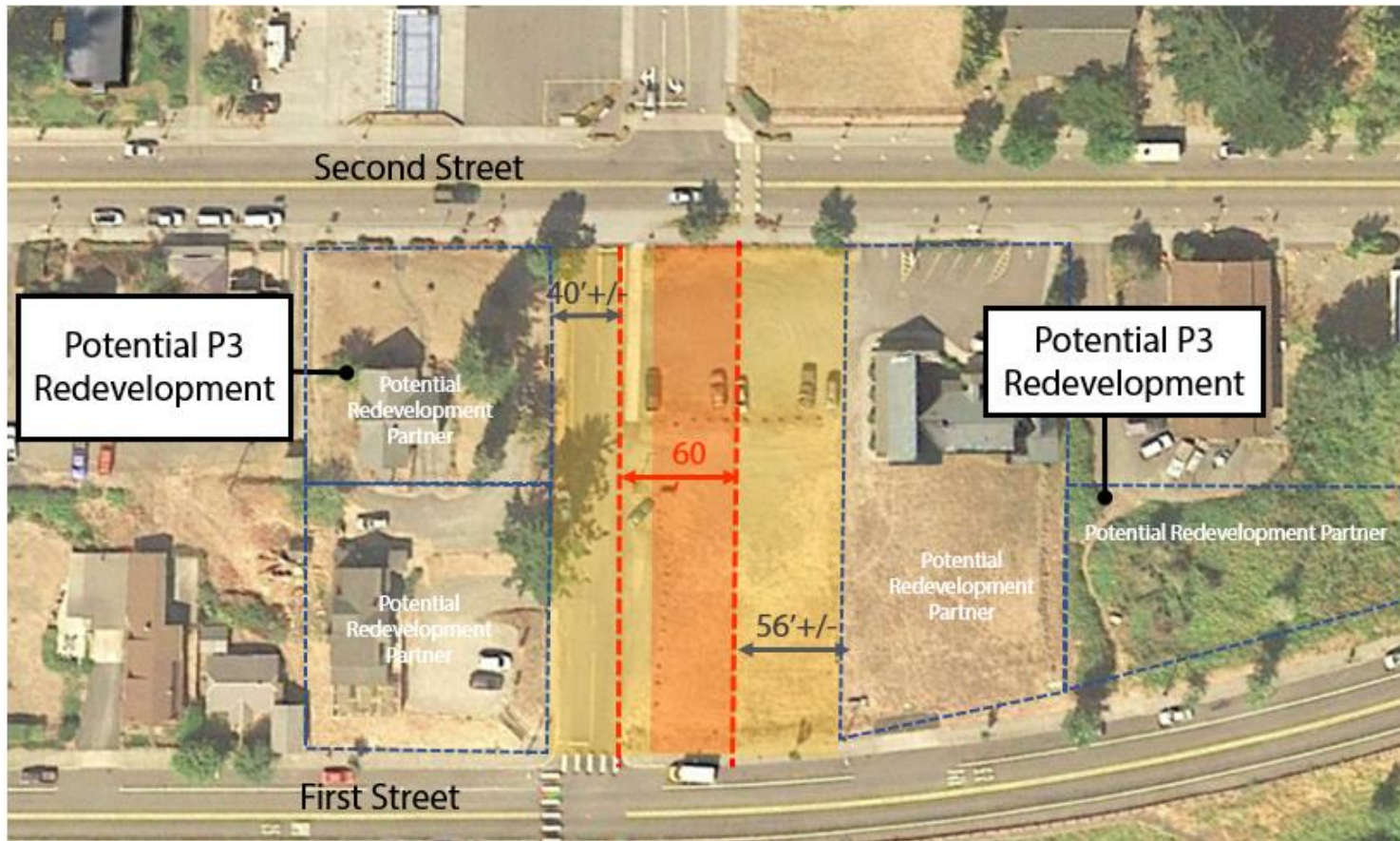
Concept

## CATALYST PROJECT

### Incentives & Public Partnerships:

*Paying **off-site infrastructure costs** and offering reduced parking requirements or in-lieu parking fees will likely be **effective incentives**.*

# Columbia Street Catalyst Project



Concept

## CATALYST PROJECT

### Incentives & Public Partnerships

Paying off-site infrastructure costs and offering reduced parking requirements or in-lieu parking fees will likely be effective incentives.

*Initial public ownership of the land* can further reduce the developer's cost while also giving the City greater ability to ensure that the project will serve public interests.

# City of Stevenson

Post Office Box 371, Stevenson, Washington 98648

Phone (509) 427-5970 Fax (509) 427-8202

October 8, 1998

Jim McClellan, P.E.  
Construction Project Engineer, SW Region  
Washington State Department of Transportation  
18102 NE 10th Avenue  
Ridgefield, WA 98642-9574

RE: C-4928, SR-14, Stevenson Two-Way Couplet  
General closure

Dear Jim:

We conducted a walk through of the project on Thursday, October 1, 1998 with City staff. No DOT or construction personnel were available. Our punch list has not changed substantially from the original list of items forwarded to your office earlier this summer. Nor does the list include any of the items that have been corrected. Obviously, the list includes only those items visible to the naked eye and detectible without instrumentation. I am assuming that DOT construction engineers will conduct a similar review of the project to ensure that the project's construction meets original design intent.

We did not individually list each location where a street sign identifying the name of the street was not replaced. Suffice it to say that there were signs at each of the intersections along First Street; and at Frank Johns, Lutheran Church Road, Seymour Street and Rock Creek Drive along Second Street. No signs were left with the City for storage during the course of construction. We are assuming that Scoccolo has these in storage and simply has not had the opportunity to replace them. We are also concerned that the regulatory signs such as stop signs may not be replaced in the proper locations. Again, we did not list these discrepancies on our punchlist since the striping has not yet been completed. And until striping is completed it will be difficult to make a final determination.

As noted in the punchlist several of the manholes have been bolted down. We were unable to check the condition of these manholes. Prior to contract closure we need to verify the condition of these manholes and secure the proper tools to release the bolts.

October 8, 1998 correspondence  
page 2

DOT has not yet arranged a walk through on the operation of the irrigation systems or the biofiltration swale on the west end of the project. We are particularly concerned that we understand proper winterization procedures on the irrigation system. With the recent failure of the biofiltration swale we are also concerned that the new plantings and barriers may not be adequately established to withstand our typical winter rains.

We are disappointed with the quality of construction on the crosswalks. The downtown crosswalks certainly can not compare to the crosswalk on the west end of Rock Creek Drive built under a former project. We have observed flaking, cracking, and uneven surfaces. But we are very reluctant to disrupt the community for another rebuild.

We appreciate your efforts to investigate our concerns regarding the pavement settlement @sta. 36+00. Your geo-technical engineer Mr. Bryan Dias recommended that a program be established to monitor the settlement and measure the rate of change. Could you forward those measurements made to date so the City has a record of the base data. We would appreciate any information or advice on the protocol that should be established for the monitoring program.

Respectfully,

Mary Ann Duncan-Cole  
City Administrator



**Washington State  
Department of Transportation**  
Sid Morrison  
Secretary of Transportation

**Southwest Region**  
Construction Project Engineer  
18102 N.E. 10th Avenue  
Ridgefield, WA 98642-9574

(360) 696-6030  
Fax (360) 696-6033

September 25, 1998

City of Stevenson  
Attention: Mary Ann Duncan-Cole  
City Manager  
P.O. Box 371  
Stevenson, WA 98648-0371

Subject: C-4928, SR-14, Stevenson Two-Way Couplet  
RE: Pavement Settlement @ sta. 36+00

Dear Mary Ann:

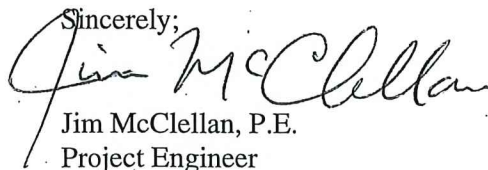
You had expressed concern about settlement of the roadway approximately 300 feet east of Columbia Street on 1<sup>st</sup> Street. The settlement occurred over the winter of 1997/1998. In response to your concern, I requested that Bryan Dias of the Olympia Service Center Geotechnical Branch investigate the site.

Bryan's observations were that the subsidence appears to be located just behind the RE-2 Retaining Wall reinforced zone. It is fairly shallow in magnitude and there is no visible distress in the retaining wall which would indicate a more severe failure. His conclusions were that the subsidence could be due to inadequate compaction, and/or possibly piping (transport of native fine material into the coarse free draining material placed behind the RE-2 Wall). As you remember, a large pool of water formed on the up hill side of 1<sup>st</sup> Street east of Columbia in the vacant lot during the winter. We have since installed a permanent pipe to drain the pond and remove the source of water.

Bryan's recommendations were to monitor the settlement to see if it continues, measure the rate of change and how it behaves over time. In the meantime, we would re-level the settlement.

At this time there has been no additional subsidence observed. Although the cause is not completely understood, it does appear that removing the primary source of water has stabilized it.

If you have any further questions, please call.

Sincerely,  
  
Jim McClellan, P.E.  
Project Engineer

JM/cah

CC: Doug Ficco  
Bryan Dias-MS 7354  
Rich Laing